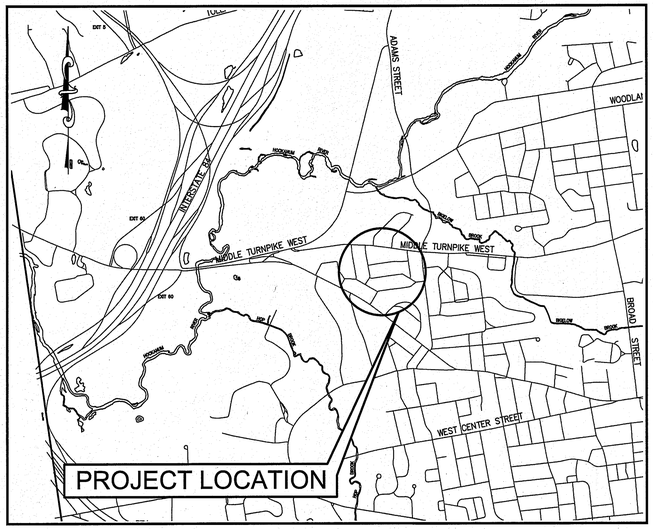
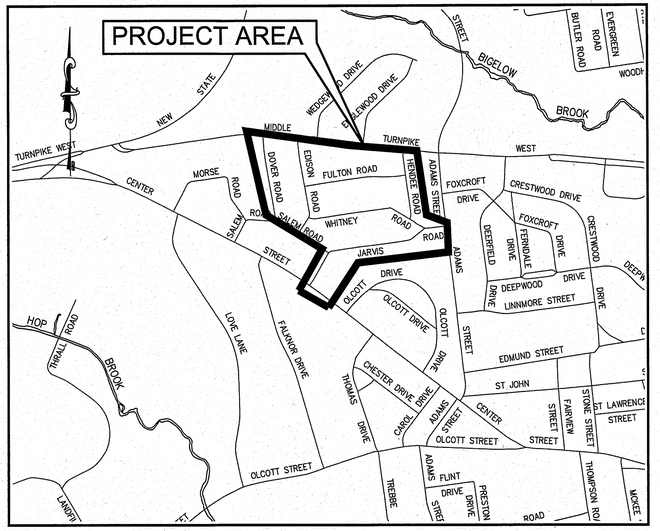


LOCATION MAP
1" = 1500'

TOWN OF MANCHESTER

PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION



SITE MAP
1" = 600'

JARVIS ROAD NEIGHBORHOOD INFRASTRUCTURE IMPROVEMENTS

AUGUST 2025

DESIGN STANDARD :	TOWN OF MANCHESTER PUBLIC IMPROVEMENT STANDARDS, EFFECTIVE DATE OCTOBER 31, 2020, AS AMENDED
DATUMS :	HORIZONTAL DATUM: TOWN OF MANCHESTER CONTROL NETWORK (NAD83 AS ESTABLISHED IN 1998) VERTICAL DATUM: TOWN OF MANCHESTER CONTROL NETWORK (NAVD88 USING GEOID 96)
STANDARD SPECIFICATIONS :	SEE CONTRACT DOCUMENTS
DESIGN SCALES :	PLAN: 1" = 20' OTHER SCALES AS NOTED

LIST OF DRAWINGS	
SHEET NO.	DESCRIPTION
1	COVER SHEET
2	TYPICAL SECTIONS AND NOTES
3-4	PLAN - JARVIS ROAD
5	PLAN - WHITNEY ROAD & HENDEE ROAD
6	PLAN - HENDEE ROAD & FULTON ROAD
7	PLAN - EDISON ROAD & FULTON ROAD
8	PLAN - DOVER ROAD & MIDDLE TURNPIKE WEST
9-11	DETAILS
N/A	CT DOT ENCROACHMENT PERMIT - PAVEMENT REPAIR
CONNECTICUT DOT STANDARD DETAILS	
TR-1210_04	PAVEMENT MARKING LINES AND SYMBOLS (08/2018)
TR-1210_08	PAVEMENT MARKINGS FOR NON-FREEWAYS (08/2018)

DESIGNED BY:
TOWN OF MANCHESTER
ENGINEERING DIVISION

STATE OF CONNECTICUT
BERNARD C. KALANSURIYA
No. 22899
LICENSED PROFESSIONAL ENGINEER

BERNARD KALANSURIYA
DESIGN ENGINEER
P.E. NO. 22899

APPROVED BY:

JOHN E. DIBIASI, P.E.
ASSISTANT TOWN ENGINEER

GENERAL NOTES:

1. ALL MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "MANCHESTER PUBLIC IMPROVEMENT STANDARDS", EFFECTIVE OCTOBER 31, 2020, AS AMENDED AND THE STATE OF CONN. DEPT. OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, FORM 819, DATED 2024, INCLUDING ANY SUPPLEMENTS.

2. IMPLEMENTING WORKER SAFETY AND HEALTH PROTOCOLS THAT ADDRESS COMPLIANCE WITH ALL RULES, LAWS AND REGULATIONS REGARDING SAFETY AND RISK OF EXPOSURE TO PHYSICAL AND CHEMICAL HAZARDS IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ALL EMPLOYEES OF THE CONTRACTOR AND SUBCONTRACTORS ARE TO WEAR REFLECTIVE VESTS AND HARD HATS AT ALL TIMES WHEN ON THE PROJECT SITE.

3. HORIZONTAL AND VERTICAL LOCATIONS OF PROPOSED WORK MAY BE ADJUSTED TO FIT EXISTING FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.

4. THE QUANTITIES AS INDICATED IN THE CONTRACT DOCUMENTS ARE APPROXIMATE AND MAY NOT INDICATE THE ACTUAL QUANTITIES OF WORK REQUIRED. THE CONTRACTOR MUST VERIFY ALL QUANTITIES.

5. THE CONTRACTOR SHALL COMMIT SUFFICIENT RESOURCES TO THE PROJECT TO ENSURE THE PROJECT IS COMPLETED WITHIN THE ALLOTTED CONTRACT TIME. ONCE MOBILIZED, THE CONTRACTOR SHALL WORK CONTINUOUSLY ON THE PROJECT UNTIL COMPLETION. ANY UNAUTHORIZED VACATING OF THE JOBSITE IS SUBJECT TO PENALTIES DESCRIBED UNDER THE "LIQUIDATED DAMAGES" SECTION OF THE CONTRACT SPECIFICATIONS.

6. THE CONTRACTOR SHALL NOT STORE CONSTRUCTION EQUIPMENT OR MATERIALS WITHIN THE PUBLIC RIGHT-OF-WAY.

7. NO STORAGE OR REFUELING OF EQUIPMENT OR VEHICLES SHALL BE CONDUCTED WITHIN THE LIMITS OF THE AQUIFER PROTECTION AREA.

8. THE CONTRACTOR SHALL CONFINE ALL OPERATIONS AND ACTIVITIES FOR CONSTRUCTION PURPOSES WITHIN THE STREET LINE UNLESS SHOWN OTHERWISE ON THE PLANS.

9. FINAL LOCATION OF ALL PROPOSED UNDERGROUND UTILITIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.

SURVEY, GIS & BASEMAPPING

10. ALL ELEVATIONS ARE BASED ON THE TOWN OF MANCHESTER CONTROL NETWORK.

11. THE CONTRACTOR SHALL TAKE CARE NOT TO DISTURB EXISTING MONUMENTATION THAT MAY BE PRESENT NEAR THE PROJECT AREA.

12. EXISTING INFORMATION SHOWN ON THESE PLANS WAS OBTAINED FROM TOWN OF MANCHESTER GIS DATA; AND THEREFORE, IS CONSIDERED APPROXIMATE. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN INFORMATION SHOWN ON THE PLANS.

PERMITTING

13. THE CONTRACTOR IS RESPONSIBLE TO OBTAIN ALL REQUIRED PERMITS AND PAY ASSOCIATED FEES PRIOR TO ANY CONSTRUCTION ACTIVITY. THE CONTRACTOR MUST OBTAIN AN ENCROACHMENT PERMIT FROM THE STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, PRIOR TO BEGINNING WORK WITHIN THE CENTER STREET (ROUTES 6 & 44) RIGHT-OF-WAY AND SHALL BE RESPONSIBLE FOR ALL ASSOCIATED FEES AND REQUIREMENTS.

14. ALL WORK SHALL BE CONDUCTED IN ACCORDANCE WITH THE EROSION CONTROL PERMIT ISSUED BY THE MANCHESTER PLANNING AND ZONING COMMISSION, WHICH WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING.

PRECONSTRUCTION

15. A PRECONSTRUCTION MEETING WITH TOWN STAFF IS REQUIRED PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITY.

16. NO WORK SHALL COMMENCE UNTIL ALL CONSTRUCTION AREA SIGNS ARE IN PLACE.

17. THE EXISTENCE OF UTILITIES AND APPURTENANCES AS SHOWN ON THESE DRAWINGS ARE FOR REFERENCE ONLY. THE EXACT SIZE, LOCATION, TYPE, AND ELEVATION OF ALL UTILITIES WITHIN ALL WORK AREAS SHALL BE THOROUGHLY INVESTIGATED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING "CALL-BEFORE-YOU-DIG" AT 1-800-922-4455 AND MUST HAVE ALL UTILITIES MARKED ON THE GROUND PRIOR TO THE START OF CONSTRUCTION.

VEHICULAR, PEDESTRIAN, & BICYCLE TRAFFIC OPERATIONS

18. THE CONTRACTOR SHALL PROVIDE TEMPORARY ACCESS TO ALL DRIVEWAYS AT ALL TIMES.

19. THE CONTRACTOR SHALL MAINTAIN TWO LANES OF TRAFFIC ON WEST MIDDLE TURNPIKE, ADAMS STREET AND CENTER STREET BETWEEN THE HOURS OF 7:00 A.M. AND 8:00 A.M. AND BETWEEN 4:30 P.M. AND 5:30 P.M. EVERY DAY. IT IS ANTICIPATED THE ROAD WILL REMAIN OPEN AT ALL TIMES WITH ALTERNATING ONE LANE TRAFFIC AND POLICE OFFICERS FOR TRAFFIC CONTROL DURING CONSTRUCTION.

ROADWAY PAVING & RECONSTRUCTION

20. THE ROAD SHALL BE RECONSTRUCTED BY REMOVING THE EXISTING PAVEMENT SURFACE AND GRADING AND COMPACTING OF GRAVEL BASE MATERIAL FOR INSTALLATION OF HMA.

21. ELEVATIONS OF NEW CONCRETE SIDEWALK/CURB AND GRASS SHELVES IN MILL AND PAVE AREAS SHALL BE DETERMINED BY THE CONTRACTOR IN THE FIELD TO PROVIDE 6" REVEAL ABOVE FINAL GUTTER ELEVATIONS THAT ARE BASED ON FINAL ROAD CENTERLINE ELEVATIONS AND PROPOSED LANE SLOPES.

22. WHEN DIRECTED BY THE ENGINEER, TOPS, FRAMES AND COVERS FOR NEW STRUCTURES LOCATED WITHIN LIMITS OF ROAD RECONSTRUCTION SHALL BE TEMPORARILY SET AT THE BINDER COURSE ELEVATION AND RAISED TO THE FINAL COURSE ELEVATION IMMEDIATELY PRIOR TO PAVING. THERE WILL BE NO ADDITIONAL PAYMENT FOR RESET OF FRAMES AND COVERS TO FINAL ROAD ELEVATIONS.

23. PROPOSED STRUCTURE FRAME ELEVATIONS IDENTIFIED ON THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL SET FRAME ELEVATIONS AS REQUIRED BASED ON EXISTING FEATURES AND GRADES IN THE VICINITY AS DIRECTED BY THE ENGINEER.

24. ALL STREETS WITH DRAINAGE WILL BE RECONSTRUCTED TO PROVIDE APPROXIMATELY 4"-5" OF REVEAL ON EXISTING GRANITE CURB. THEREFORE, DRAINAGE STRUCTURE FRAME ELEVATIONS SHOULD BE SET 5.5" BELOW TOP OF CURB ELEVATIONS UNLESS DIRECTED OTHERWISE BY THE ENGINEER. TOP OF FRAME ELEVATIONS IDENTIFIED ON THE PLAN ARE APPROXIMATE.

25. STREET CORNERS AND SIDEWALK RAMPS SHALL BE CONSTRUCTED TO PROVIDE POSITIVE DRAINAGE TOWARDS THE NEAREST CATCH BASIN TO PREVENT PONDING OF STORMWATER. SIDEWALK RAMPS MUST BE INSTALLED TO MATCH PROPOSED GUTTER ELEVATIONS WHICH SHOULD PROVIDE 4"-5" OF REVEAL ON EXISTING GRANITE CURB ADJACENT TO RAMPS.

STORMWATER, STORM SEWERS & CULVERTS

26. THE CONTRACTOR SHALL SUBMIT A PLAN TO THE ENGINEER FOR PROPOSED METHODS TO DEWATER THE SITE (WHERE REQUIRED). SUCH PLAN SHALL INCLUDE THE INSTALLATION OF SILT BAGS AT PUMP DISCHARGES WITH SILT FENCE/HAYBALES FOR EROSION CONTROL. PROPOSED DISCHARGE LOCATIONS AND ALL EROSION CONTROLS SHALL BE REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.

27. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY HANDLING OF ALL STORMWATER RUNOFF DURING CONSTRUCTION. METHODS OF HANDLING RUNOFF SHALL BE APPROVED BY THE ENGINEER.

28. AT THE END OF EACH WORKING DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING NEW DRAINAGE SYSTEMS TO EXISTING. ALL DRAINAGE SYSTEMS WITHIN THE CONSTRUCTION LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR.

29. ALL NEW CATCH BASINS SHALL BE TYPE "C" WITH GRANITE CURB INLETS (0'-10' DEEP) UNLESS NOTED OTHERWISE. GRANITE CURB INLETS SHALL BE INSTALLED WHERE GRANITE CURB EXISTS AND CONCRETE CURB INLETS THAT MATCH CURB DIMENSIONS SHALL BE INSTALLED WHERE BITUMINOUS CONCRETE CURB OR CONCRETE CURB EXISTS. CATCH BASINS WITH 3-SIDED TOPS SHALL BE USED WHERE REQUIRED CURB REVEAL IS NOT ATTAINABLE AS SHOWN ON THE PLANS OR WHERE DIRECTED BY THE ENGINEER (TYPICALLY INSTALLED ADJACENT TO CONCRETE DRIVEWAY APRONS SIDEWALK RAMPS).

30. THE CONTRACTOR SHALL NOT ABANDON ANY EXISTING DRAINAGE PIPES UNLESS CALLED FOR ON THE PLANS OR DIRECTED BY THE ENGINEER. PIPES THAT ARE TO BE ABANDONED SHALL BE BULKHEADED AT EACH END AND COMPLETELY FILLED WITH FLOWABLE CONCRETE AS DIRECTED BY THE ENGINEER (PAY ITEM: ABANDON PIPE). IF PIPE HAS A BLIND TIE-IN TO THE EXISTING MAIN, THEN PIPE SHALL BE BULKHEADED AT THE UPSTREAM END ONLY (PAY ITEM: PLUG PIPE).

WATER & SANITARY SEWER UTILITIES

31. FOR CONNECTIONS TO EXISTING SANITARY SEWER STRUCTURES AND PIPING, THE CONTRACTOR SHALL VERIFY EXISTING INVERT ELEVATIONS, NOTIFY THE ENGINEER IF A DISCREPANCY EXISTS, AND ADJUST THE PIPE SLOPES AS DIRECTED.

32. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY HANDLING OF ALL SEWAGE FLOWS DURING CONSTRUCTION. METHODS OF HANDLING SEWAGE FLOWS SHALL BE APPROVED BY THE ENGINEER.

33. AT THE END OF EACH WORKING DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING EXISTING SANITARY SEWER LATERALS TO NEW SANITARY FACILITIES. THE CONTRACTOR IS RESPONSIBLE FOR PROPER DISPOSAL OF ALL MATERIALS.

34. ALL SANITARY SEWER MANHOLES SHALL HAVE A 48" INTERNAL DIAMETER UNLESS SPECIFIED OTHERWISE ON THE PLANS.

35. AT ALL WATER AND SEWER UTILITY CROSSINGS, A MINIMUM 18" VERTICAL AND 10' HORIZONTAL SEPARATION DISTANCE SHALL BE PROVIDED UNLESS INDICATED OTHERWISE ON THE PLANS OR APPROVED BY THE ENGINEER. IF THIS SEPARATION DISTANCE CANNOT BE ACHIEVED, THE ENGINEER SHALL BE NOTIFIED PRIOR TO PROCEEDING.

36. AS DIRECTED BY THE ENGINEER, A CONCRETE PIPE CRADLE SHALL BE INSTALLED WHEN THE VERTICAL CLEARANCE BETWEEN WATER, SANITARY SEWER, DRAINAGE AND OTHER UTILITY PIPING IS LESS THAN OR EQUAL TO 12" AND CRUSHED STONE SHALL BE USED WHENEVER THE VERTICAL CLEARANCE IS GREATER THAN 12" BUT LESS THAN OR EQUAL TO 18", IN ACCORDANCE WITH THE ASSOCIATED UTILITY SUPPORT DETAIL.

37. ALL EXISTING HYDRANTS AND FRAMES AND COVERS FROM SANITARY SEWER MANHOLES TO BE REMOVED, REPLACED OR ABANDONED SHALL BE SALVAGED. ALL OTHER MATERIALS WHICH ARE REMOVED FROM THE SITE BECOME PROPERTY OF THE CONTRACTOR, AND ARE TO BE DISPOSED OF PROPERLY, UNLESS INDICATED OTHERWISE ON THE PLANS. ALL MATERIALS THE ENGINEER DESIGNATES TO BE SALVAGED SHALL BE DELIVERED TO THE WATER & SEWER DEPARTMENT GARAGE ON CHARTER OAK STREET, MANCHESTER, CT AND PROPERLY UNLOADED BY THE CONTRACTOR.

38. ALL VALVE BOXES AND CURB BOXES SHALL BE ADJUSTED TO MATCH FINISHED GRADES. ALL CURB BOXES SHALL BE LOCATED IN GRASSED AREAS AT THE STREET LINE FRONTING THE PROPERTY UNLESS INDICATED OTHERWISE ON THE PLANS.

39. ALL NEW WATER MAINS AND SERVICES SHALL HAVE 4.5 FEET OF COVER UNLESS OTHERWISE INDICATED ON THE PLANS. COVER LESS THAN OR IN EXCESS OF 4.5 FEET SHALL BE ALLOWED ONLY WHERE INDICATED ON THE PLANS OR APPROVED BY THE ENGINEER. WATER MAINS AND SERVICES WITH COVER LESS THAN 4.5 FEET SHALL BE INSULATED UNLESS APPROVED OTHERWISE BY THE ENGINEER.

40. THRUST RESTRAINT FOR ALL MECHANICAL JOINTS AT VALVES AND FITTINGS SHALL BE PROVIDED BY MEANS OF DUCTILE IRON RESTRAINER GLANDS. WEDGE-ACTION JOINT RESTRAINERS OR GASKET-TYPE JOINT RESTRAINT SHALL BE USED TO RESTRAIN ALL DUCTILE IRON PIPE JOINTS FOR A DISTANCE OF AT LEAST 27 FEET ON EACH SIDE OF ALL VALVES OR FITTINGS. NO MORE THAN ONE PIPE JOINT SHALL BE ALLOWED WITHIN THAT 27 FEET OF PIPE.

41. ALL EXISTING WATER SERVICES SHALL BE RECONNECTED TO THE NEW FACILITIES. ALL WATER SERVICES SHALL BE NEW 1" COPPER TUBING FROM THE MAIN TO THE CURB BOX UNLESS OTHERWISE INDICATED ON THE PLANS. THE CURB BOX SHALL BE REPLACED AS PART OF THE INSTALLATION.

42. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY TEMPORARY THRUST RESTRAINT THAT IS REQUIRED.

43. ALL VALVES AND HYDRANTS SHALL BE RIGHT-OPENING (CLOCKWISE) UNLESS INDICATED OTHERWISE. ALL MAIN LINE AND AUXILIARY VALVES (4" TO AND INCLUDING 12") SHALL BE RESILIENT WEDGE GATE VALVES.

44. EXISTING WATER AND SEWER MAINS INDICATED ON THE PLANS TO BE ABANDONED SHALL BE REMOVED WHEN SUCH MAINS ARE LOCATED WITHIN THE HORIZONTAL TRENCH LIMITS OF NEW PIPE TO BE INSTALLED IN THE SAME LOCATION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE LINEAR FOOT COST FOR THE ASSOCIATED PIPE INSTALLATION.

45. WHEN DIRECTED BY THE ENGINEER, TEMPORARY FIRE HYDRANTS SHALL BE PROVIDED WHEN EXISTING HYDRANTS ARE OUT OF SERVICE DUE TO CONSTRUCTION. THERE WILL BE NO SEPARATE PAYMENT FOR TEMPORARY HYDRANTS, THEIR COST SHALL BE INCLUDED IN THE BID PRICE FOR "HYDRANT ASSEMBLY" OF THE TYPE SPECIFIED.

46. EVEN WHEN NOT SPECIFICALLY LABELED ON THE CONTRACT PLANS, EXISTING SIDEWALKS, DRIVEWAYS, DRIVEWAY APRONS, CURBING AND LAWN AREAS DISTURBED DURING INSTALLATION OF SANITARY SEWER AND WATER FACILITIES SHALL BE REPAIRED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS AS DIRECTED BY THE ENGINEER DURING CONSTRUCTION AND PAID FOR UNDER THE ASSOCIATED CONTRACT ITEMS.

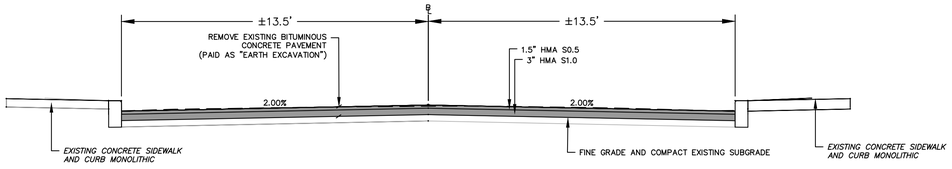
RESTORATION, EROSION & SEDIMENTATION CONTROL

47. ALL SEDIMENT CONTROL SYSTEMS SHALL MEET THE "GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" AS PREPARED BY THE CONNECTICUT COUNCIL ON SOIL AND WATER CONSERVATION, LATEST REVISION. THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION, MAINTENANCE AND REPAIR OF EROSION CONTROLS REQUIRED FOR THE PROJECT. ADDITIONAL EROSION CONTROLS SHALL BE INSTALLED BY THE CONTRACTOR FOR TEMPORARY STOCKPILING OF EXCAVATED MATERIAL AND WHERE DEEMED NECESSARY BY THE ENGINEER. EROSION CONTROLS SHALL REMAIN IN PLACE AND BE MAINTAINED BY THE CONTRACTOR UNTIL THE SITE IS STABILIZED AND THE ENGINEER APPROVES THEIR REMOVAL.

48. SILT SACKS SHALL BE INSTALLED IN ALL EXISTING AND PROPOSED CATCH BASINS WITHIN THE PROJECT AREA AND WHERE DIRECTED BY THE ENGINEER. SILT SACKS SHALL BE THE APPROPRIATE TYPE FOR CATCH BASINS WITH AND WITHOUT CURB INLETS.

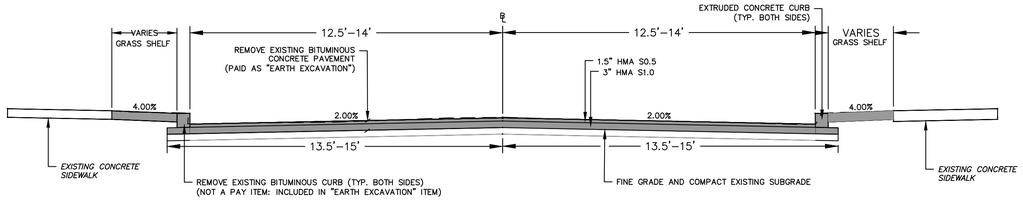
49. ALL GRASSED AREAS DISTURBED BY THE CONTRACTOR SHALL BE REPLACED WITH TOPSOIL, FERTILIZED AND SEEDED AS PER THE SPECIFICATIONS. CONTRACTOR SHALL MAKE ALL EFFORTS TO MINIMIZE THE LIMITS OF DISTURBANCE AND ASSOCIATED RESTORATION THAT IS REQUIRED.

50. ANY DRIVEWAYS, SIDEWALKS, CURB AND LAWN AREAS LOCATED ON PRIVATE PROPERTY OR WITHIN THE RIGHT-OF-WAY THAT ARE IMPACTED DURING CONSTRUCTION SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS AS IDENTIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE REQUIRED LIMITS OF SUCH RESTORATION SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. RESTORATION ON PRIVATE PROPERTY SHALL BE COMPLETED AS PROMPTLY AS PRACTICAL WITHIN THIRTY (30) CALENDAR DAYS OF COMPLETING WORK ON THE PROPERTY.



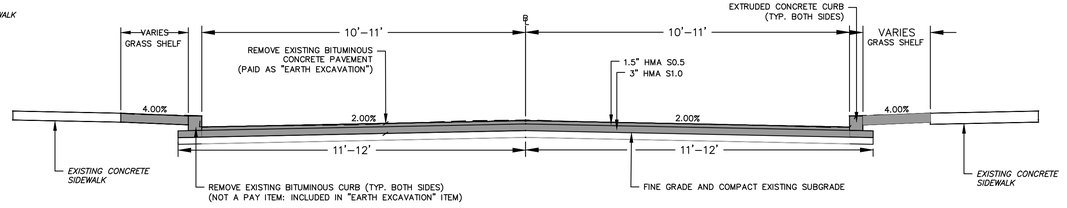
TYPICAL SECTION - EDISON RD, FULTON RD, HENDEE RD (STA 0+00 TO 3+50±)

NOT TO SCALE



TYPICAL SECTION - JARVIS RD, DOVER RD, HENDEE RD (STA 3+50± TO STA 5+75±)

NOT TO SCALE



TYPICAL SECTION - WHITNEY RD

NOT TO SCALE

TOWN of MANCHESTER CONNECTICUT SEAL

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION

494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

WETLANDS BOUNDARY	LIGHT POLE
RETAINING WALL	CONIFEROUS TREE
GUIDE RAIL	DECIDUOUS TREE
STONE WALL	SANITARY MANHOLE
STOCKADE FENCE	DRAINAGE MANHOLE
WIRE FENCE	CATCH BASIN
CHAIN LINK FENCE	CULVERT END
PROPERTY LINE	HYDRANT
RAILROAD TRACKS	CURB STOP
SILT FENCE	WATER VALVE
CONCRETE MONUMENT	BUTTERFLY VALVE
GRANITE MONUMENT	BLOW OFF
IRON PIPE	SIGN
IRON ROD	DOUBLE POST SIGN
CONTROL POINT	MAIL BOX
DRILL HOLE	BOLLARD
UTILITY POLE	CONTROLLER CABINET
UTILITY POLE WITH LIGHT	GAS GATE
TRAFFIC SPAN POLE	TELEPHONE BOX
ELECTRIC BOX	CATV TUBE
WETLAND FLAG	

PROJECT NUMBER
2023099

FILENAME
2023099PLAN.DWG

NO.	DATE	FILE
-	08/07/25	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JD
RELEASED BY: JL

DRAWING SCALE
NTS

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
TYPICAL SECTIONS AND NOTES

SHEET NUMBER
2 of 11

R:\Project Public Works\2023099 - Water Main Replacements - Jarvis Neighborhood Proj\2023099PLAN.dwg, 8/11/2025 5:13:07 PM, tkulanowitz

CONNECT TO EXISTING 6" WATER MAIN USING SOLID SLEEVES
JARVIS ROAD STA 0+00 BEGIN ROAD RESURFACING CUT CONCRETE PAVEMENT
8" GATE VALVE (TYP.OF 2)
R20.0'
8"X6" DI REDUCER
8"X8" DI TEE
8" MJ PLUG
R25.0'
HYDRANT ASSEMBLY
ABANDON VALVE
1" COPPER SERVICE
ABANDON WATER MAIN IN PLACE
EXTRUDED CONCRETE CURB
8" CLDIP
SMH-1
T.F.=±130.0
INV.=±123.3 (MATCH EXISTING)
RECONNECT COPPER SERVICE (TYP.)
RESET VALVE BOX (TYP.OF 3)
R35.0'
CONNECT TO EXISTING 8" CLDIP USING SOLID SLEEVE
BITUMINOUS CONCRETE DRIVEWAY APRON (TYP. OF ALL DRIVEWAYS ON JARVIS ROAD)
TYPE "C" CATCH BASIN
T.F.=±127.2
INV IN (N)=±124.9 (MATCH EXISTING)
INV OUT=±124.9 (MATCH EXISTING)
1" COPPER SERVICE/REPLUMB METER
PAVEMENT REPAIR (STATE ROAD)
REMOVE EXISTING BITUMINOUS CURB ON BOTH SIDES OF STREET (INCLUDED IN EARTH EXCAVATION ITEM)
LIMIT OF ROAD RESURFACING CUT BITUMINOUS CONCRETE PAVEMENT
RESET MANHOLE (SANITARY SEWER)
CONNECT TO EXISTING 8" CLDIP USING SOLID SLEEVE
8" X 1/8 DI BEND
RESET TYPE "C" CATCH BASIN TOP (NEW TOP)
T.F.=±127.2
REMOVE EXISTING BITUMINOUS CONCRETE PAVEMENT FORM AND COMPACT EXISTING SUBGRADE INSTALL 1.5" HMA S0.5 OVER 3" HMA S1.0
8" CLDIP INSTALL 2' MIN FROM EXISTING MAIN
RECONNECT COPPER SERVICE (TYP.OF BOTH SIDES)
EXTRUDED CONCRETE CURB
RESET MANHOLE (SANITARY SEWER)
MATCHLINE - STA 6+00
SEE SHEET 4
Center Street (Conn. Route 6 & 44)
Jarvis Road
Salem Road
Concrete Sidewalk
Bituminous Curb
SEPTIC

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

WETLANDS BOUNDARY	LIGHT POLE
RETAINING WALL	CONIFEROUS TREE
GUIDE RAIL	DECIDUOUS TREE
STONE WALL	SANITARY MANHOLE
STOCKADE FENCE	DRAINAGE MANHOLE
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WETLAND FLAG	

PROJECT NUMBER
2023099

FILENAME
2023099PLAN.DWG

NO.	DATE	FILE
–	08/07/25	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JD
RELEASED BY: JL

DRAWING SCALE
HORIZONTAL: 1" = 20' VERTICAL: ---
OR AS NOTED
20 10 0 20
GRAPHIC SCALE

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION

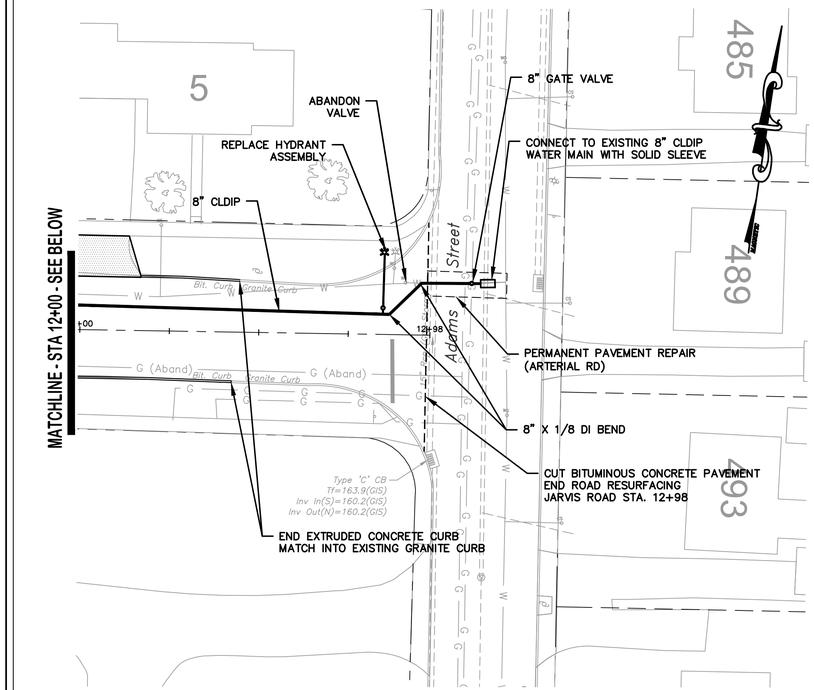
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

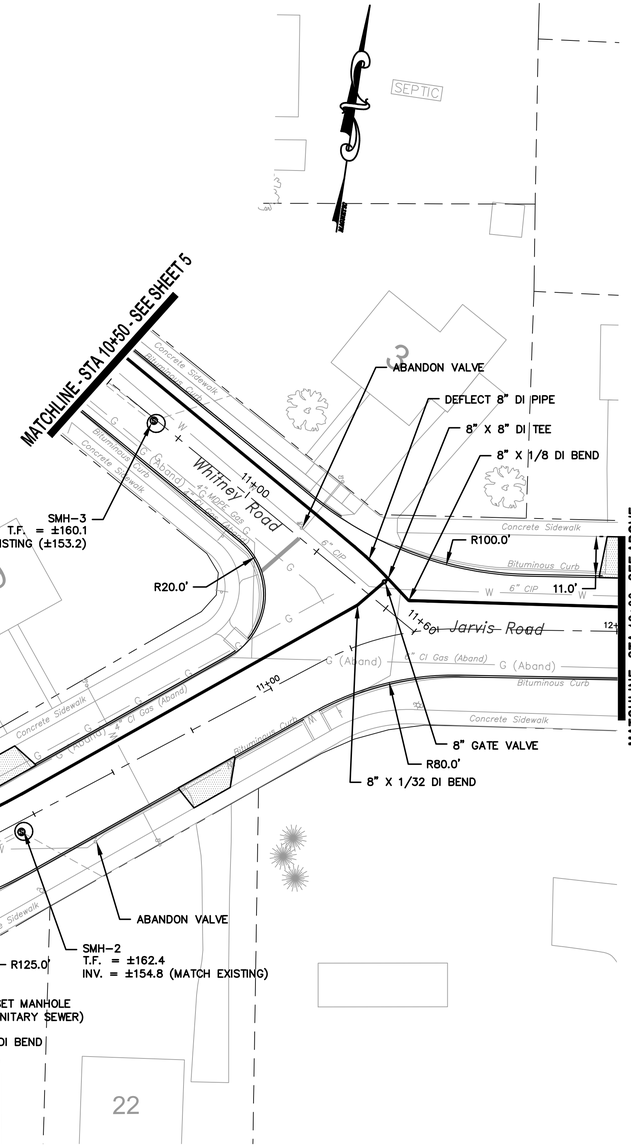
PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
PLAN
JARVIS ROAD

SHEET NUMBER
3 of 11

R:\Project\Public Works\2023099 - Water Main Replacements - Jarvis Neighborhood\Proj\2023099PLAN.dwg, 8/11/2025 4:34:42 PM, Mahanuraya





TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

= WETLANDS BOUNDARY	= LIGHT POLE
= RETAINING WALL	= CONIFEROUS TREE
= GUIDE RAIL	= DECIDUOUS TREE
= STONE WALL	= SANITARY MANHOLE
= STOCKADE FENCE	= DRAINAGE MANHOLE
= WIRE FENCE	= CATCH BASIN
= CHAIN LINK FENCE	= CULVERT END
= PROPERTY LINE	= HYDRANT
= RAILROAD TRACKS	= CURB STOP
= SILT FENCE	= WATER VALVE
= CONCRETE MONUMENT	= BUTTERFLY VALVE
= GRANITE MONUMENT	= BLOW OFF
= IRON PIPE	= SIGN
= IRON ROD	= DOUBLE POST SIGN
= CONTROL POINT	= MAIL BOX
= DRILL HOLE	= BOLLARD
= UTILITY POLE	= CONTROLLER CABINET
= UTILITY POLE WITH LIGHT	= GAS GATE
= TRAFFIC SPAN POLE	= TELEPHONE BOX
= ELECTRIC BOX	= CATV TUBE
= WETLAND FLAG	

PROJECT NUMBER
2023099

FILENAME
2023099PLAN.DWG

NO.	DATE	FILE
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CHECKED BY: JD
RELEASED BY: JL

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OR AS NOTED
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GRAPHIC SCALE

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
PLAN
JARVIS ROAD

SHEET NUMBER
4 of 11

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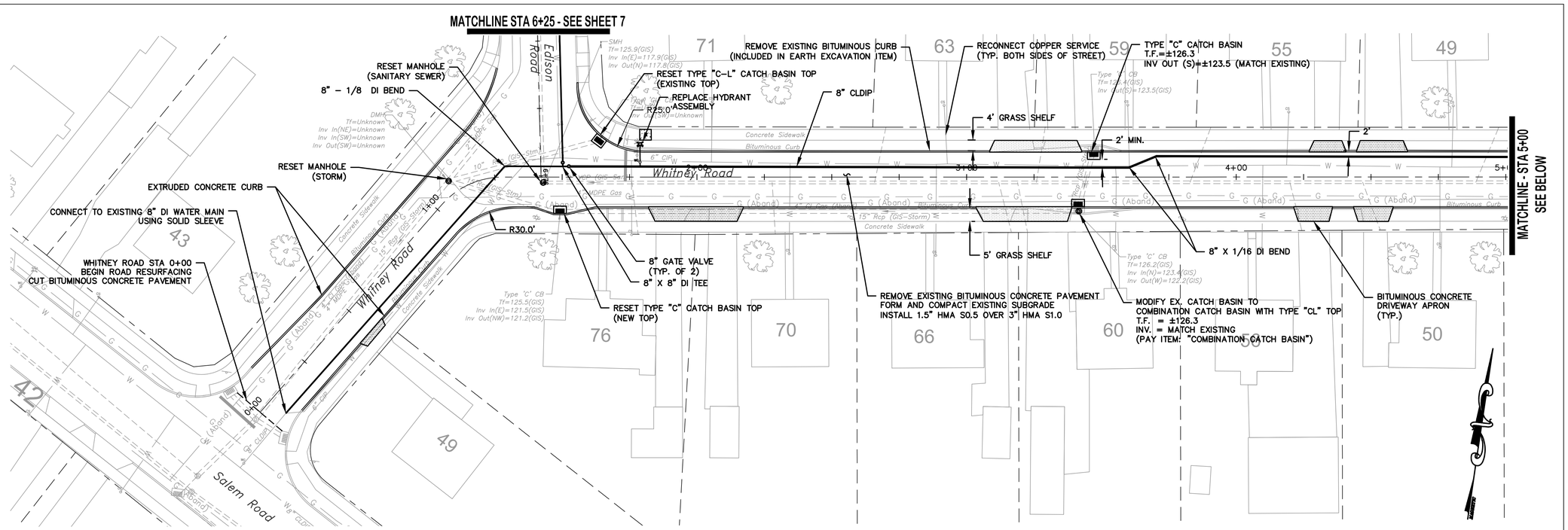

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045–0191

LEGEND

- WETLANDS BOUNDARY
- RETAINING WALL
- GUIDE RAIL
- STONE WALL
- STOCKADE FENCE
- WIRE FENCE
- CHAIN LINK FENCE
- PROPERTY LINE
- RAILROAD TRACKS
- SILT FENCE
- CONCRETE MONUMENT
- GRANITE MONUMENT
- IRON PIPE
- IRON ROD
- CONTROL POINT
- DRILL HOLE
- UTILITY POLE
- UTILITY POLE WITH LIGHT
- TRAFFIC SPAN POLE
- ELECTRIC BOX
- WETLAND FLAG
- LIGHT POLE
- CONIFEROUS TREE
- DECIDUOUS TREE
- SANITARY MANHOLE
- DRAINAGE MANHOLE
- CATCH BASIN
- CULVERT END
- HYDRANT
- CURB STOP
- WATER VALVE
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PROJECT LOCATION
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
PLAN
WHITNEY RD & HENDEE RD

SHEET NUMBER
5 of 11

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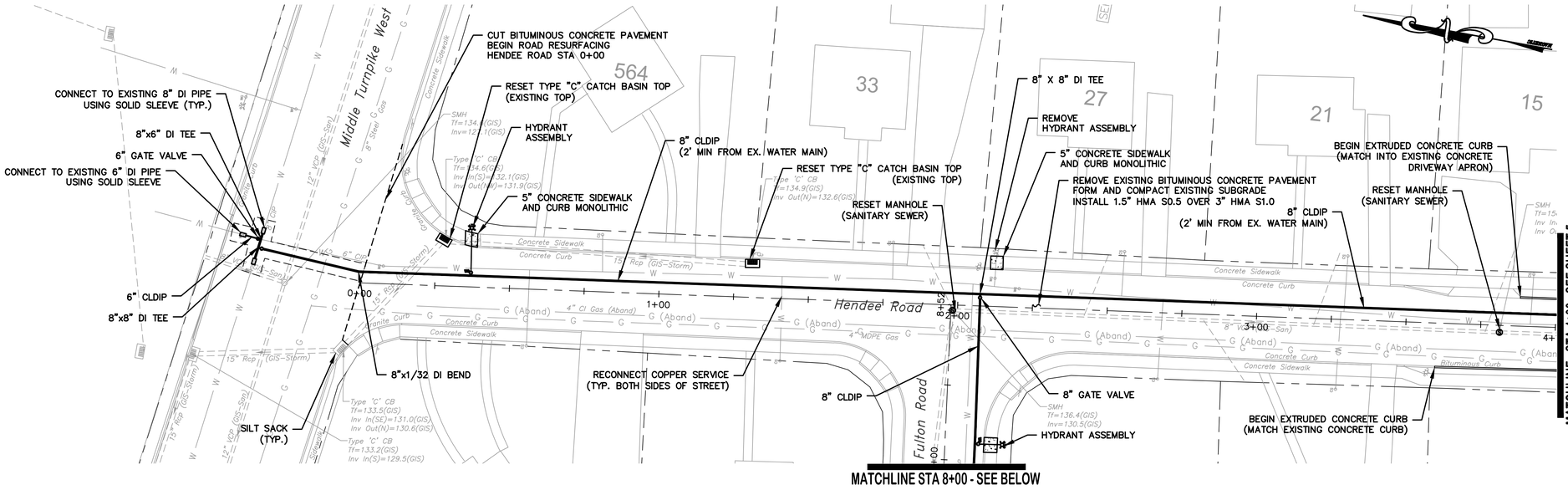

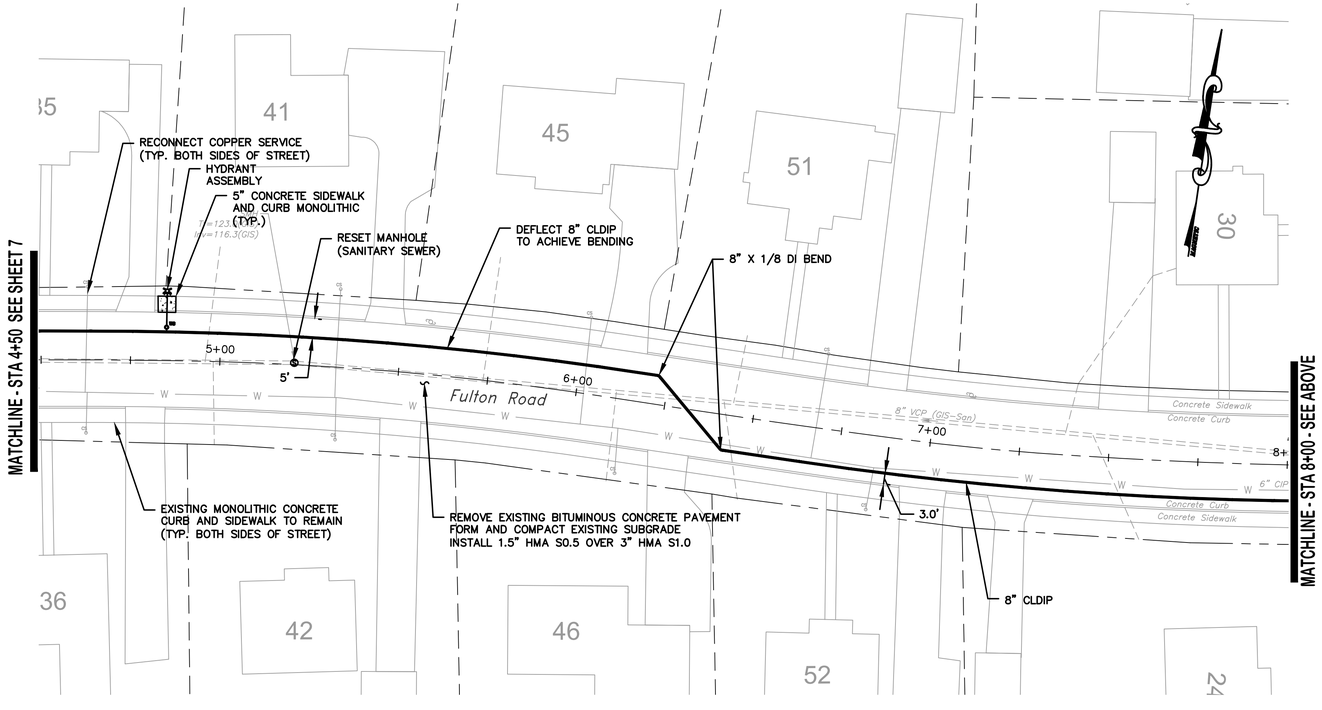

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045–0191

LEGEND

- WETLANDS BOUNDARY
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PROJECT NUMBER
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PROJECT LOCATION
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HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
PLAN
HENDEE RD & FULTON RD

SHEET NUMBER
6 of 11

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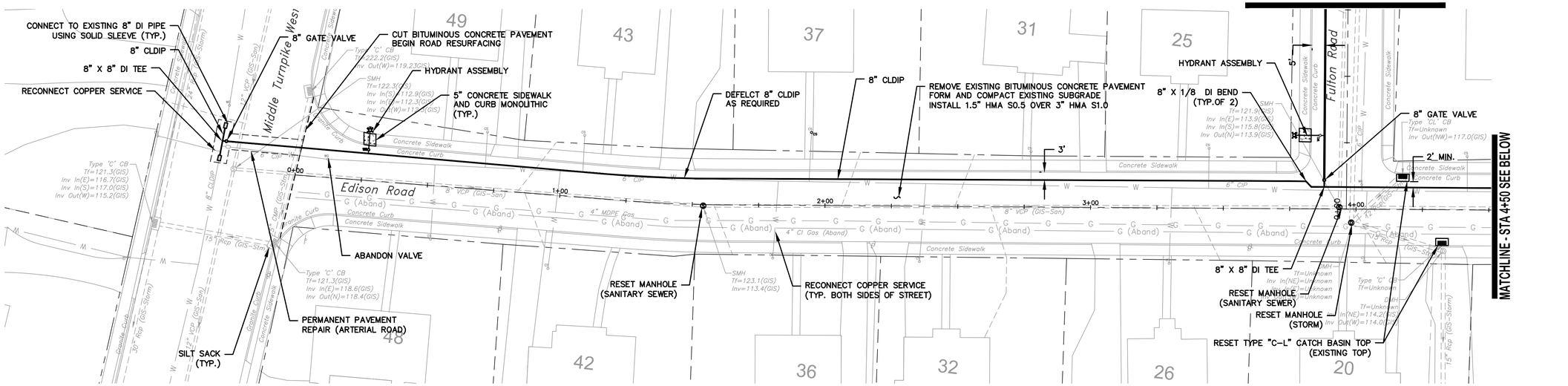

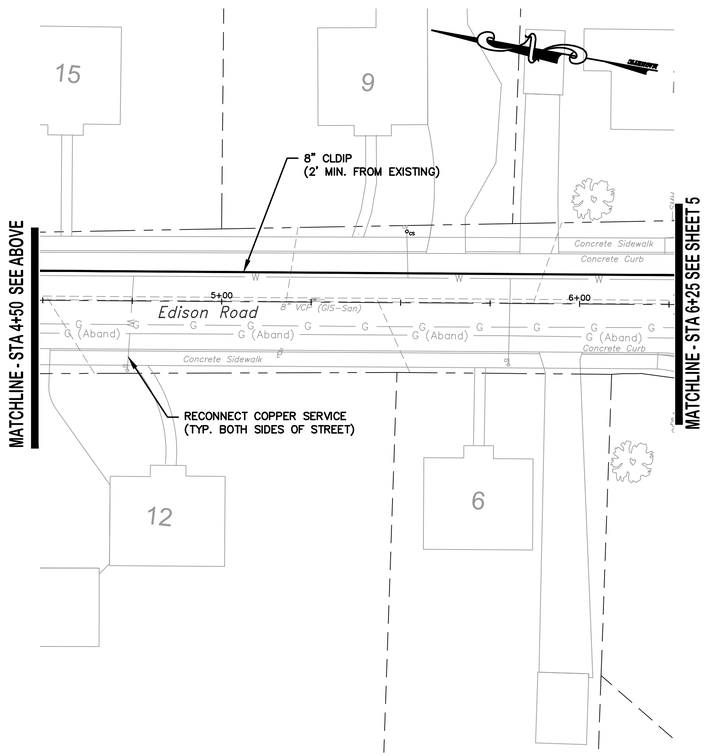

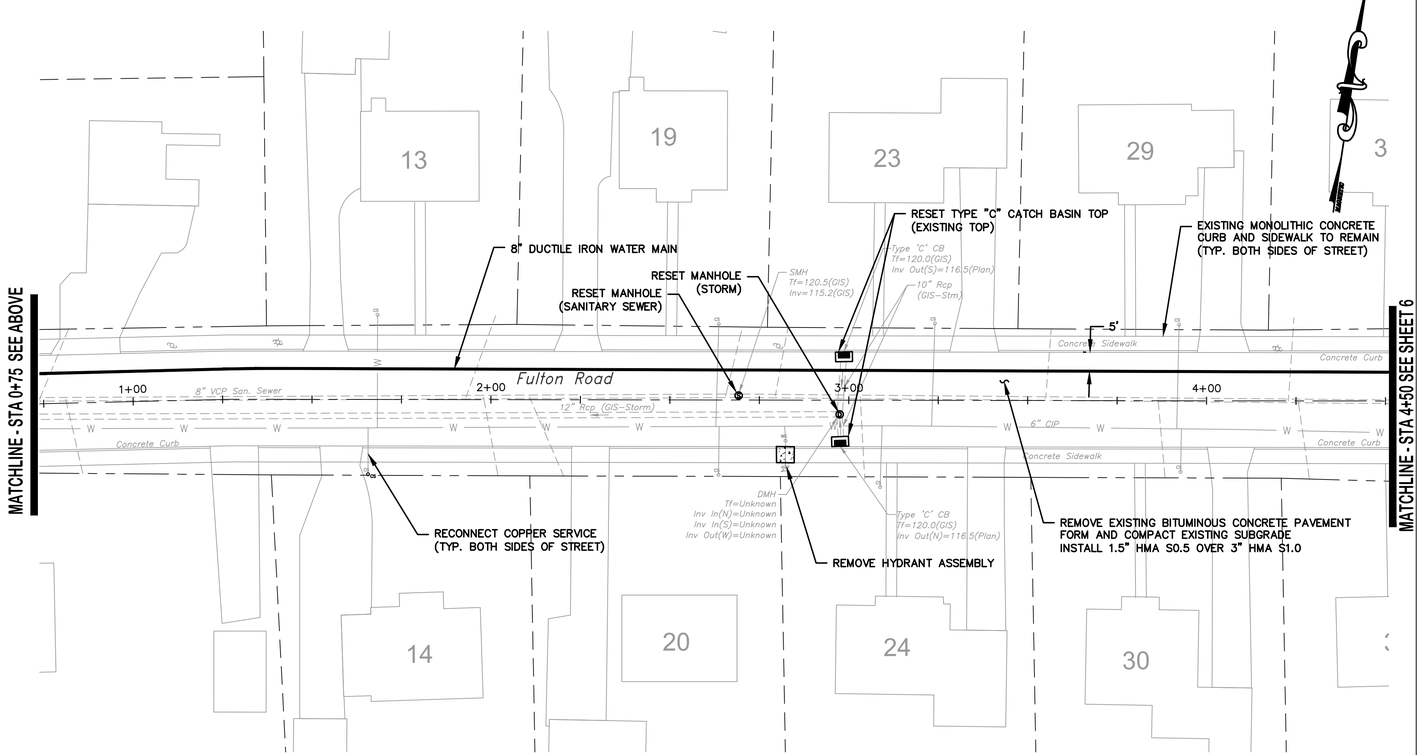

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

- WETLANDS BOUNDARY
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PROJECT NUMBER
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GRAPHIC SCALE

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PROJECT LOCATION
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
PLAN
EDISON RD & FULTON RD

SHEET NUMBER
7 of 11

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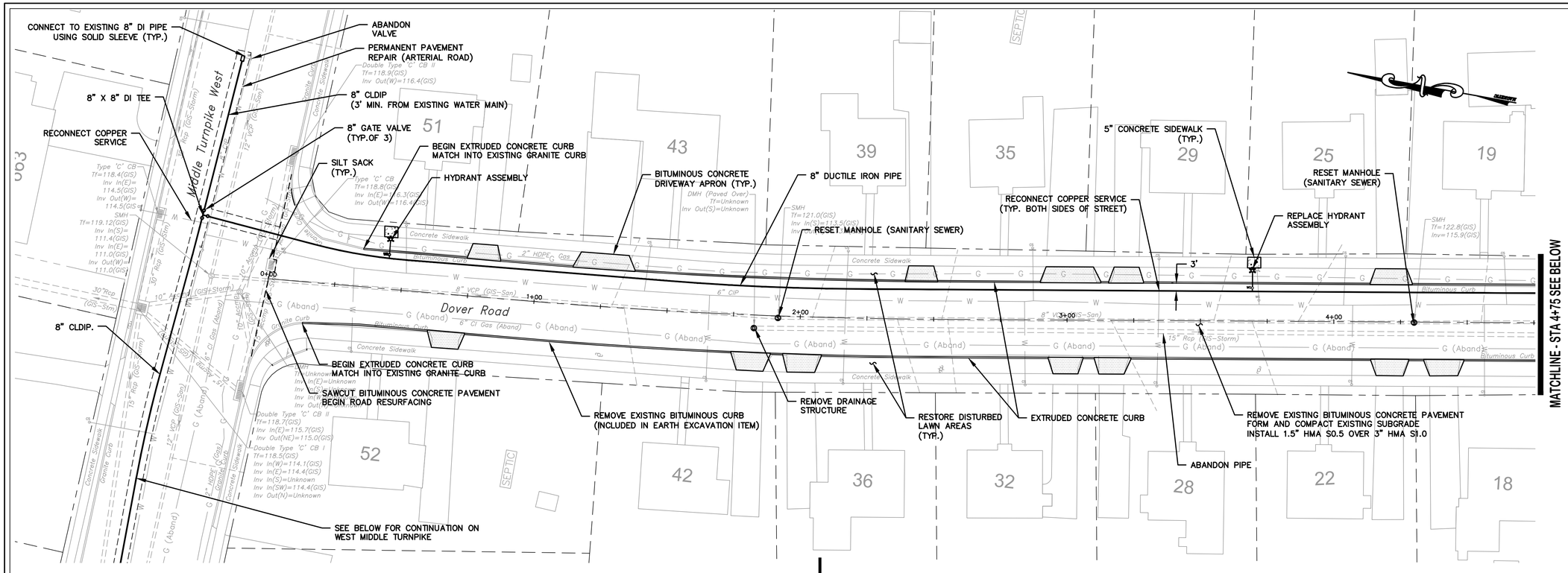

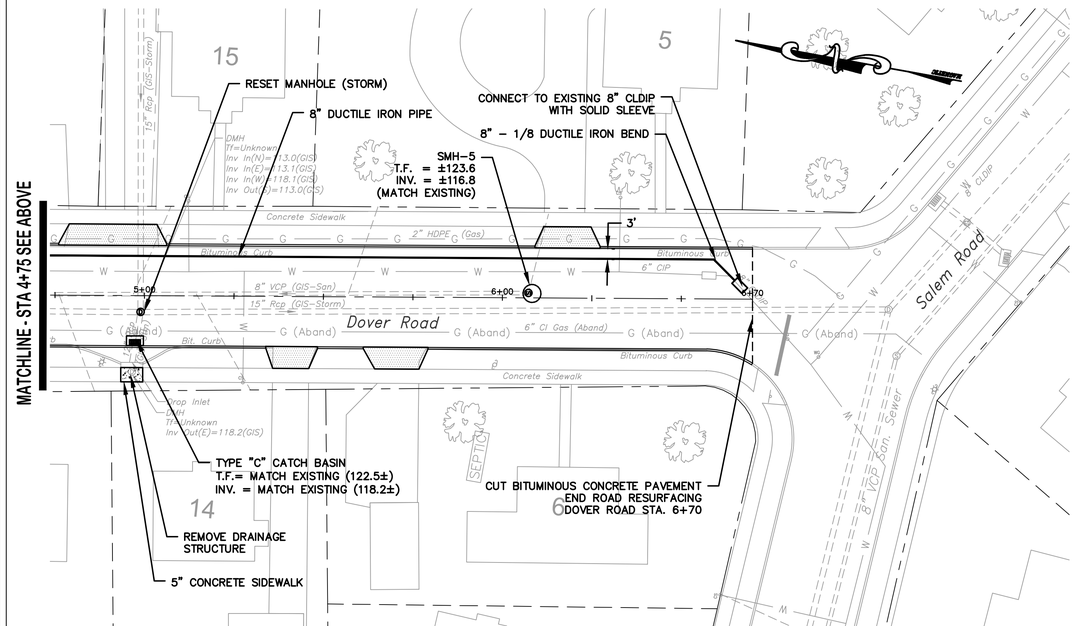

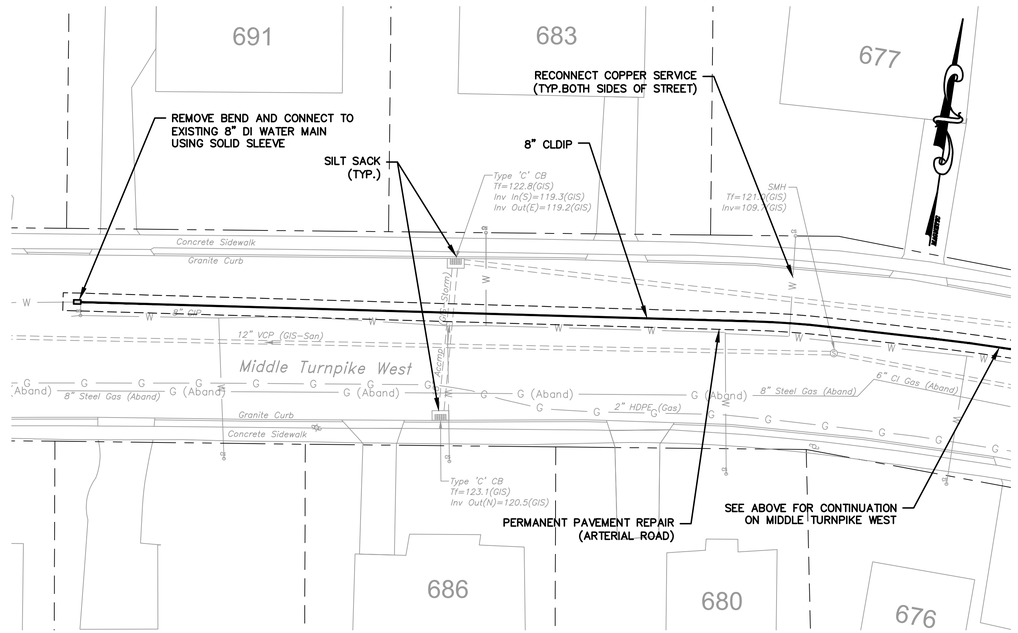

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045–0191

LEGEND

WETLANDS BOUNDARY	LIGHT POLE
RETAINING WALL	CONIFEROUS TREE
GUIDE RAIL	DECIDUOUS TREE
STONE WALL	SANITARY MANHOLE
STOCKADE FENCE	DRAINAGE MANHOLE
WIRE FENCE	CATCH BASIN
CHAIN LINK FENCE	CULVERT END
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ELECTRIC BOX	CATV TUBE
WETLAND FLAG	

PROJECT NUMBER
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DATUM
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PROJECT LOCATION
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
PLAN
DOVER RD & MIDDLE TURNPIKE WEST

SHEET NUMBER
8 of 11

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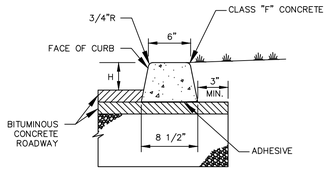
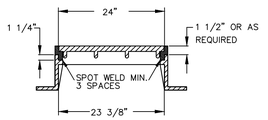


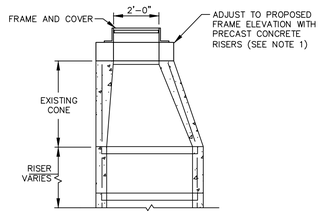
NOTE
CURB HEIGHT (H) SHALL BE AS SPECIFIED ON THE PLANS.

EXTRUDED CONCRETE CURB

NOT TO SCALE



MANHOLE RISER RING

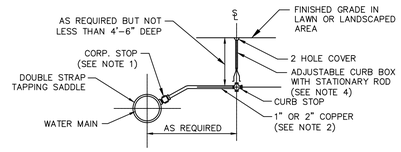


MANHOLE VERTICAL SECTION

NOTE:
1. CONCRETE BLOCKS OR BRICKS MAY BE USED AS DIRECTED BY THE ENGINEER WHERE FRAME IS TO BE SET AT FINISHED GRADE.

RESET MANHOLE

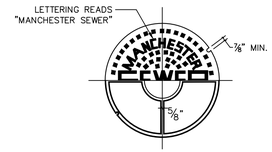
NOT TO SCALE

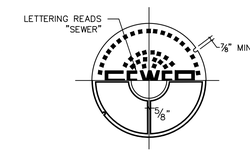


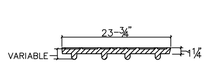
NOTES:
1. THE TOP OF THE CORPORATION AND THE FIRST THREE (3) FEET OF COPPER TUBING SHALL BE INSTALLED NO HIGHER THAN THE TOP OF THE WATER MAIN.
2. NO INTERMEDIATE SIZES (i.e. 3/4", 1 1/2", 1 3/4") ARE ALLOWED FOR COPPER SERVICES. ANY SERVICE REQUIREMENT GREATER THAN 2" COPPER SHALL BE CLDIP (4" MIN.) WITH THE SHUT-OFF LOCATED AT THE MAIN. COPPER TUBING SHALL BE CONTINUOUS WITH NO COUPLINGS BETWEEN THE CORPORATION STOP AND THE CURB STOP.
3. IN GENERAL, ALL EXISTING SERVICES THAT ARE CONSTRUCTED OF MATERIALS OTHER THAN COPPER TUBING BETWEEN THE CURB STOP AND METER SHALL BE REPLACED UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
4. TOP SECTION OF VALVE BOX WITH FLANGE SHALL BE SET AT FINISHED GRADE OVER CURB STOP COVER WHEN LOCATED WITHIN PAVED AREAS AND SIDEWALK.

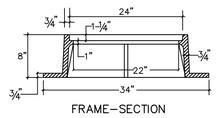
TYPICAL WATER SERVICE CONNECTION

NOT TO SCALE







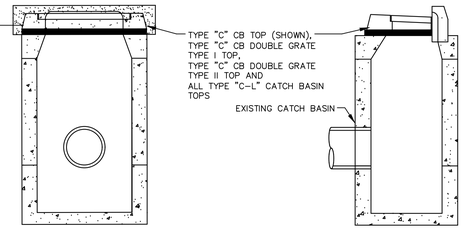


NOTES:
1. MANHOLE FRAMES AND COVERS SHALL BE THE MODEL AND MANUFACTURER LISTED IN THE CONTRACT SPECIFICATIONS.
2. BOLTS FOR BOLTED COVERS SHALL BE ½" STAINLESS STEEL.

SANITARY SEWER MANHOLE FRAME AND COVER

NOT TO SCALE

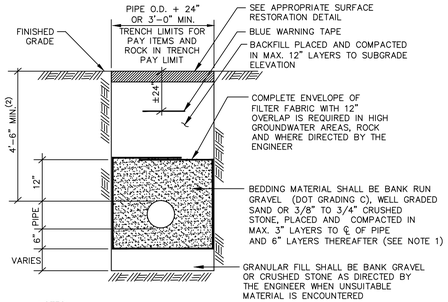
GENERAL DESCRIPTION OF WORK:
1. SAWCUT AND REMOVE EXISTING BITUMINOUS CONCRETE PAVEMENT AND BASE MATERIAL UP TO THE TOP OF THE STRUCTURE.
2. REMOVE EXISTING CATCH BASIN TOP.
3. RECONSTRUCT STRUCTURE WALLS TO A 3' DEPTH UNLESS OTHERWISE DIRECTED BY THE TOWN.
4. PROVIDE AT LEAST ONE ROW OF CONCRETE BRICKS/BLOCKS OR PRECAST CONCRETE RISER (1 1/2" MIN. THICKNESS) TO ADJUST THE NEW CB TOP TO MATCH THE NEW PAVEMENT SURFACE.
5. NEW PAVEMENT COURSES AND PROCESSED AGGREGATE BASE THICKNESS SHALL MATCH EXISTING OR BE INSTALLED AS DIRECTED BY THE TOWN. PROCESSED AGGREGATE BASE THICKNESS SHALL BE 12" MINIMUM.



NOTES:
1. WHEN CONCRETE BLOCKS ARE USED, MAXIMUM CORBEL IS 2" PER COURSE OF BLOCK.
2. WALLS SHALL BE CONCRETE BLOCK OR PRECAST CONCRETE SECTIONS.
3. WALL THICKNESS TO BE 12" WHEN TOTAL HEIGHT OF STRUCTURE EXCEEDS 10' FROM TOP OF FRAME TO BOTTOM OF BASE.
4. THIS DETAIL SHOWN RESETTING A NEW TYPE "C" CATCH BASIN TOP WITH GRANITE CURB INLET; HOWEVER, IT SHALL BE USED FOR ALL CATCH BASIN TYPES SPECIFIED ON THE PLANS.

RESET CATCH BASIN TOP

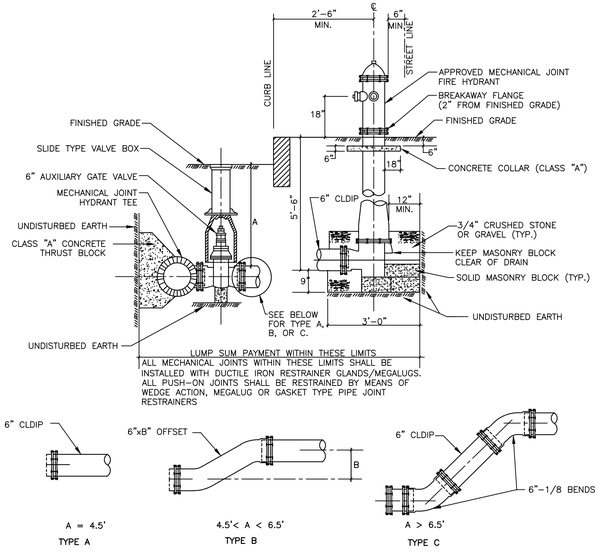
NOT TO SCALE



NOTES:
1. CRUSHED STONE SHALL ONLY BE USED IN HIGH GROUNDWATER CONDITIONS AS DIRECTED BY THE ENGINEER.
2. ALL WATER MAIN WITH LESS THAN 4'-6" OF COVER SHALL BE INSULATED UNLESS APPROVED OTHERWISE BY THE ENGINEER. SEE TYPICAL TRENCH DETAIL (INSULATED WATER) FOR ADDITIONAL INFORMATION.

TYPICAL TRENCH DETAIL (WATER)

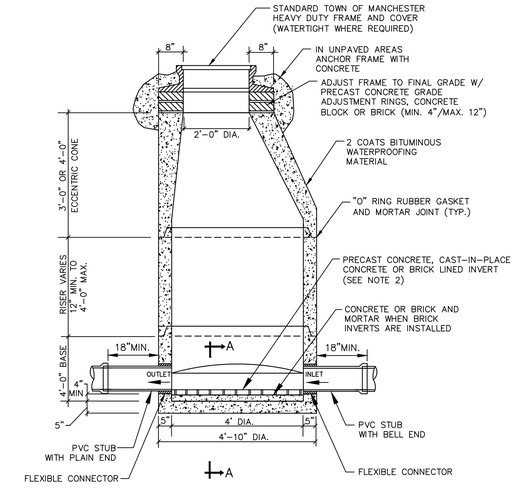
NOT TO SCALE



NOTES:
1. TYPE "A" HYDRANT HAS NO OFFSETS OR BENDS BETWEEN THE AUXILIARY VALVE AND THE HYDRANT.
2. THE REQUIRED TYPE SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

HYDRANT ASSEMBLY

NOT TO SCALE



SECTION A-A

NOTES:
1. MAXIMUM PIPE SIZE TO BE USED IN 4' DIA. MANHOLE IS 24".
2. BRICK INVERT CONSTRUCTION IS SHOWN; HOWEVER, PRECAST OR CAST-IN-PLACE CONCRETE INVERTS SHALL BE INSTALLED UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

48" SANITARY MANHOLE

NOT TO SCALE

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

WETLANDS BOUNDARY	LIGHT POLE
RETAINING WALL	CONIFEROUS TREE
GUIDE RAIL	DECIDUOUS TREE
STONE WALL	SANITARY MANHOLE
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TRAFFIC SPAN POLE	TELEPHONE BOX
ELECTRIC BOX	CATV TUBE
WETLAND FLAG	

PROJECT NUMBER
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2023099PLAN.DWG

NO.	DATE	FILE
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DRAWING SCALE
NTS

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JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
DETAILS

SHEET NUMBER
9 of 11

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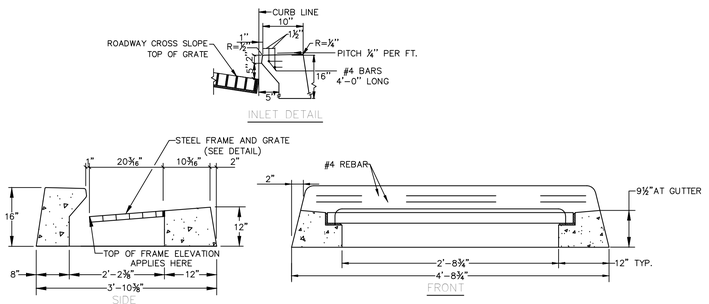
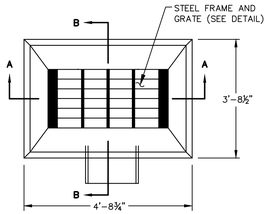
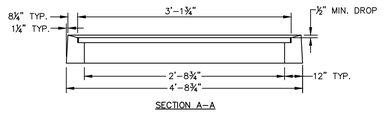
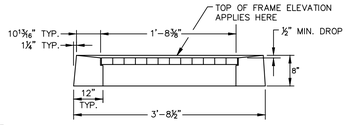


NOTES:
1. CONCRETE TOP SHALL BE CAST TO MATCH ADJACENT CONCRETE CURB.
2. CATCH BASIN TOP DIMENSIONS PROVIDED ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

TYPE "C" CATCH BASIN TOP
(FOR CONCRETE CURB)
NOT TO SCALE







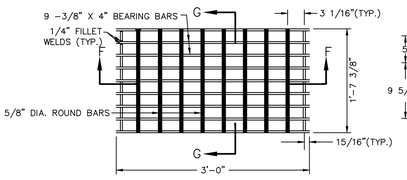
NOTE:
1. CATCH BASIN TOP DIMENSIONS PROVIDED ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

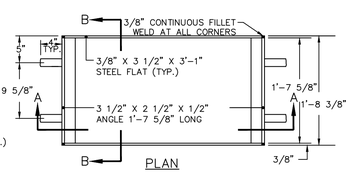
TYPE "C-L" CATCH BASIN TOP
NOT TO SCALE

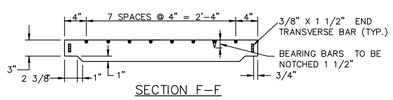
CATCH BASIN NOTES:

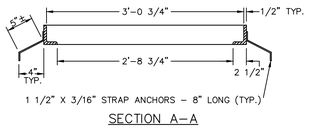
1. REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60.
2. CATCH BASIN DETAILS SHOW STANDARD REINFORCEMENT. WELDED WIRE FABRIC WITH AN AREA EQUAL TO OR GREATER THAN THE REINFORCING SHOWN MAY BE SUBSTITUTED.
3. ALL LAP SPLICES, DEVELOPMENT LENGTHS, BENDS FOR REINFORCEMENT, AND WELDED WIRE FABRIC SHALL CONFORM TO AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.
4. ALL REINFORCEMENT SHALL HAVE A MINIMUM CLEAR COVER OF 2".
5. MINIMUM CONCRETE COMPRESSIVE STRENGTH Fc' = 4000 PSI SHALL BE OBTAINED PRIOR TO SHIPPING.
6. BASES AND RISERS AT A DEPTH OF 20' OR MORE SHALL BE DESIGNED BY THE CONTRACTOR AND WORKING DRAWINGS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW.
7. SEE APPROPRIATE DETAIL FOR CATCH BASIN FRAMES AND GRATES.
8. RISERS MAY BE PREFABRICATED WITH PIPE OPENINGS IN ALL FOUR WALLS. ADEQUATE REINFORCING AROUND PIPE OPENINGS CONFORMING TO THESE PLANS SHALL BE PROVIDED. ANY RISERS USED WHERE A PIPE OPENING IS TO REMAIN IN PLACE, MUST BE FORMED UP WITH BRICK AS DIRECTED BY THE TOWN.
9. RISERS SHALL NEVER HAVE CORNER PIPE ENTRIES.
10. ALL OPENINGS SURROUNDING PIPES SHALL BE CLOSED USING CEMENT RUBBLE MASONRY CONFORMING TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M6.06. IF THE TOWN DETERMINES THAT THE CLOSURE OF ANY PIPE OPENING IS UNSATISFACTORY, THE CONTRACTOR SHALL IMMEDIATELY RECLOSE SAID OPENING. THE LOCATION OF PIPE OPENINGS SHALL NOT REDUCE THE WALL THICKNESS.
11. THE LATEST CONNECTICUT D.O.T. STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
12. WALL THICKNESS OF ALL CATCH BASINS OVER 10' DEEP SHALL BE INCREASED TO 12" THICK. INSIDE DIMENSION SHALL REMAIN THE SAME (THE 12" THICKNESS WILL START AFTER THE FIRST 10').
13. BUTYL RUBBER JOINT SEAL SHALL CONFORM TO AASHTO M–198 AND MORTAR SHALL CONFORM TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M11.04. ALL CATCH BASINS SHALL BE BUILT WITH SPACERS AS SHOWN IN THE DETAILS.
14. SHRINKAGE AND TEMPERATURE REINFORCEMENT SHALL BE PROVIDED IN THE TOPS OF SLABS. THE TOTAL AREA OF REINFORCEMENT PROVIDED SHALL BE AT LEAST 0.125 SQUARE INCHES PER FOOT IN EACH DIRECTION. THE MAXIMUM SPACING OF THIS REINFORCEMENT SHALL NOT EXCEED 18 INCHES.
15. ALL CONCRETE CATCH BASIN TOPS SHALL BE CAST TO MATCH ADJACENT CURB TYPE AND HEIGHT.
16. CATCH BASIN TOP DIMENSIONS PROVIDED ON THE PLANS ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

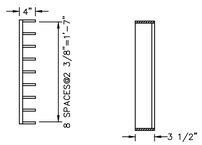
CATCH BASIN NOTES
NOT TO SCALE





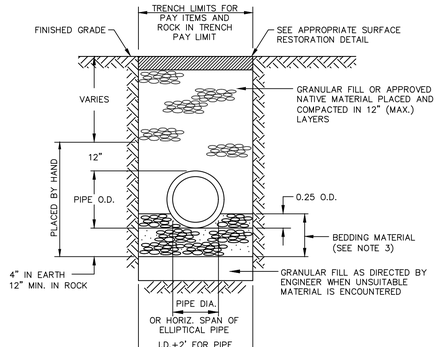






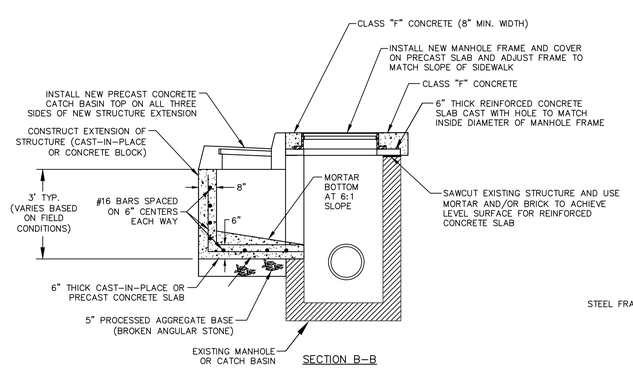
NOTES:
1. ALL BARS SHALL BE WELDED AT ALL INTERSECTIONS.
2. FRAMES AND GRATES SHALL BE GALVANIZED IN ACCORDANCE WITH THE LATEST STATE OF CONNECTICUT SPECIFICATIONS MATERIAL SECTION M.06.03.

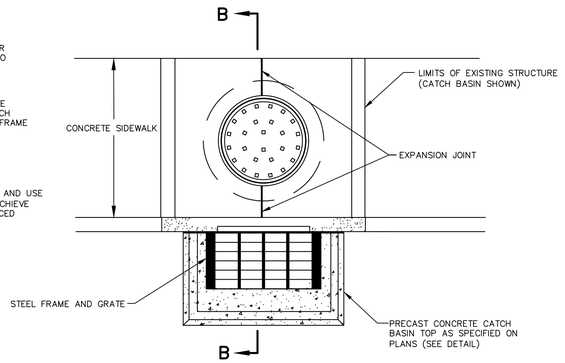
STEEL FRAME AND GRATE
NOT TO SCALE



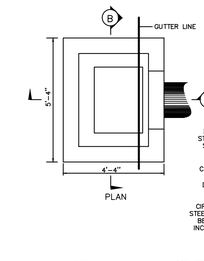
NOTES:
1. ALL CONCRETE PIPE TO BE MINIMUM CLASS IV WITH 2 FT OF COVER UNLESS OTHERWISE APPROVED BY THE ENGINEER.
2. USE WATERTIGHT RUBBER GASKETS IN ALL PIPE JOINTS.
3. BEDDING MATERIAL SHALL BE INSTALLED A MINIMUM OF 12" ABOVE THE TOP OF ALL PLASTIC PIPES AND PIPES 48" IN DIAMETER AND LARGER.
4. EXCAVATION, BEDDING MATERIAL AND BACKFILL ARE INCLUDED IN CONTRACT UNIT PRICE BID FOR "CULVERT" OF THE TYPE SPECIFIED.

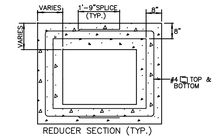
TYPICAL TRENCH DETAIL
(STORM SEWER)
NOT TO SCALE

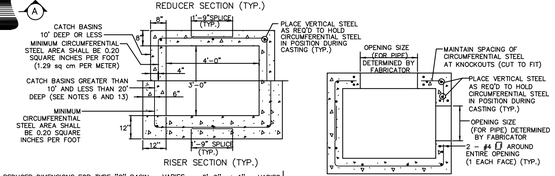


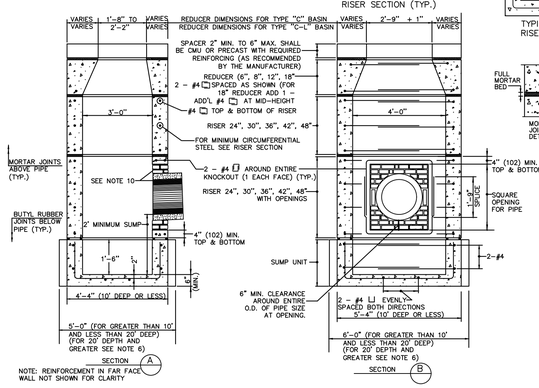


COMBINATION CATCH BASIN
NOT TO SCALE









TYPE "C" AND TYPE "C-L" CATCH BASIN
NOT TO SCALE

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045–0191

LEGEND

WETLANDS BOUNDARY	LIGHT POLE
RETAINING WALL	CONIFEROUS TREE
GUIDE RAIL	DECIDUOUS TREE
STONE WALL	SANITARY MANHOLE
STOCKADE FENCE	DRAINAGE MANHOLE
WIRE FENCE	CATCH BASIN
CHAIN LINK FENCE	CULVERT END
PROPERTY LINE	HYDRANT
RAILROAD TRACKS	CURB STOP
SILT FENCE	WATER VALVE
CONCRETE MONUMENT	BUTTERFLY VALVE
GRANITE MONUMENT	BLOW OFF
IRON PIPE	SIGN
IRON ROD	DOUBLE POST SIGN
CONTROL POINT	MAIL BOX
DRILL HOLE	BOLLARD
UTILITY POLE	CONTROLLER CABINET
UTILITY POLE WITH LIGHT	GAS GATE
TRAFFIC SPAN POLE	TELEPHONE BOX
ELECTRIC BOX	CATV TUBE
WETLAND FLAG	

PROJECT NUMBER
2023099

FILENAME
2023099PLAN.DWG

NO.	DATE	FILE
–	08/07/25	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JD
RELEASED BY: JL

DRAWING SCALE
NTS

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
DETAILS

SHEET NUMBER
10 of 11

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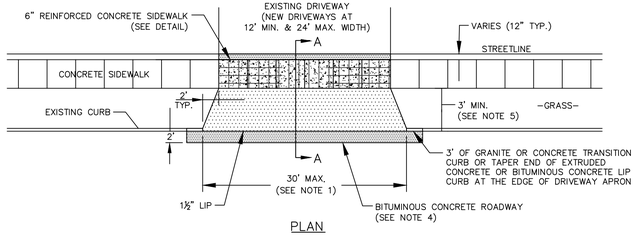
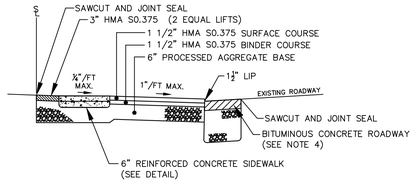


PLAN

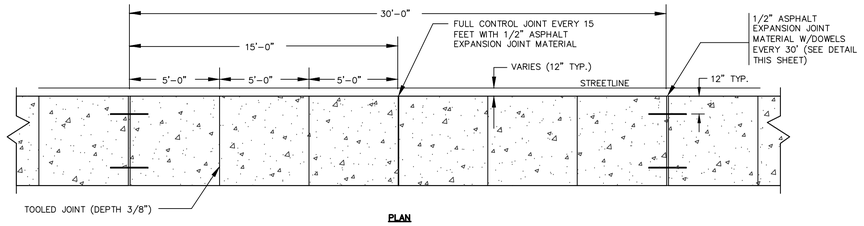


SECTION A–A

NOTES:
1. MAXIMUM WIDTH OF APRON IS 30' AT CURB LINE. THE TOWN MAY ALLOW WIDER APRON WIDTHS TO ACCOMMODATE EXISTING ACCESS CONDITIONS AND/OR REQUIRED VEHICLE TURNING MOVEMENTS
2. MINIMUM DISTANCE BETWEEN MULTIPLE DRIVEWAY OPENINGS SHALL BE 10'.
3. SEE RESPECTIVE DETAILS FOR CONCRETE SIDEWALK AND CURB.
4. PAVEMENT COURSES AND PROCESSED AGGREGATE BASE THICKNESS SHALL MATCH EXISTING OR BE INSTALLED AS DIRECTED BY THE ENGINEER. PROCESSED AGGREGATE BASE THICKNESS SHALL BE 12" MINIMUM.
5. CONCRETE DRIVEWAY APRON EXTENDING FROM BACK OF SIDEWALK TO ROAD GUTTER SHALL BE INSTALLED WHERE SHELF IS LESS THAN 3 FT WIDE. SEE APPROPRIATE DETAIL.

BITUMINOUS CONCRETE DRIVEWAY APRON
(PAY ITEM: "BITUMINOUS CONCRETE DRIVEWAY")

NOT TO SCALE



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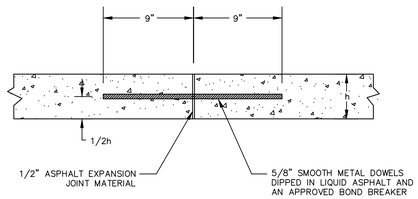
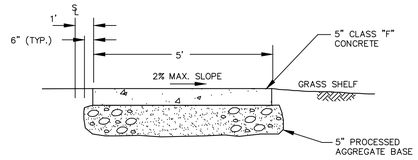


EXPANSION JOINT – DETAIL

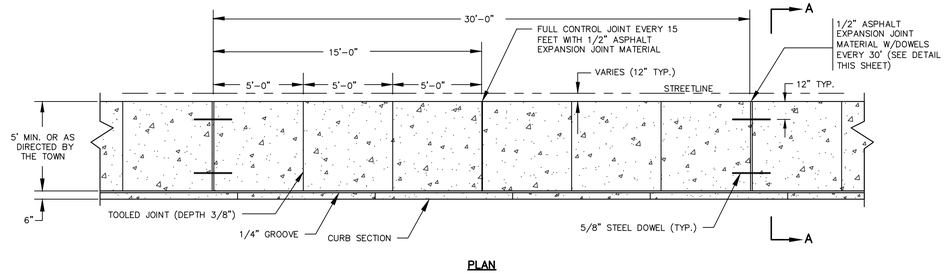


SECTION

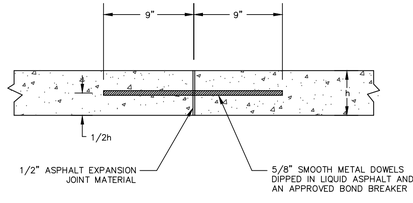
NOTES:
1. A MINIMUM 36" CLEAR WIDTH SHALL BE MAINTAINED BETWEEN THE EDGES OF THE SIDEWALK AND ANY OBSTRUCTIONS WITHIN THE SIDEWALK LIMITS.
2. AT THE END OF THE DAILY POUR OF CONCRETE, METAL DOWELS ARE TO BE INSERTED IN THE LAST SLAB FOR THE EXTENSION OF THE SIDEWALK.
3. INSTALL APPROVED BOND BREAKER BETWEEN GRANITE CURB AND SIDEWALK.

5" CONCRETE SIDEWALK

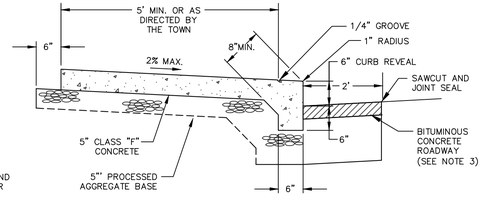
NOT TO SCALE



PLAN



EXPANSION JOINT – DETAIL

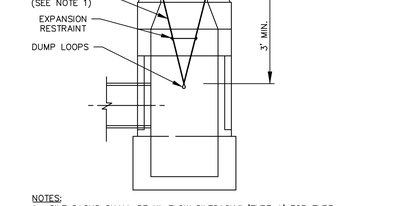


SECTION A–A

NOTES:
1. A MINIMUM 36" CLEAR WIDTH SHALL BE MAINTAINED BETWEEN THE EDGES OF THE SIDEWALK AND ANY OBSTRUCTIONS WITHIN THE SIDEWALK LIMITS.
2. AT THE END OF THE DAILY POUR OF CONCRETE, METAL DOWELS ARE TO BE INSERTED IN THE LAST SLAB FOR THE EXTENSION OF THE SIDEWALK.
3. PAVEMENT COURSES AND PROCESSED AGGREGATE BASE THICKNESS SHALL MATCH DETAIL OR BE INSTALLED AS DIRECTED BY THE ENGINEER.

5" CONCRETE SIDEWALK AND CURB MONOLITHIC

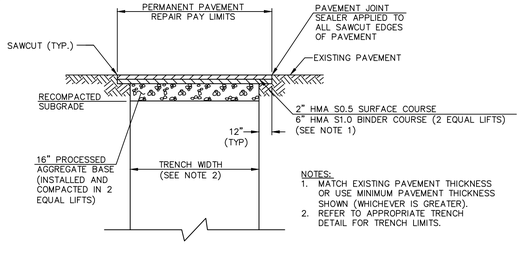
NOT TO SCALE



NOTES:
1. SILT SACKS SHALL BE HI–FLOW SILTSACK® 'TYPE A' FOR TYPE "C–L" CB TOPS AND 'TYPE B' WITH CURB DEFLECTORS FOR TYPE "C" CB TOPS OR OTHER STRUCTURES WITH CURB INLETS AS MANUFACTURED BY ACF ENVIRONMENTAL, INC OR APPROVED EQUAL.
2. SILT SACKS SHALL BE PROVIDED WITH INTERNAL OVERFLOWS.
3. SILT SACKS SHALL BE EMPTIED WHEN THEY HAVE COLLECTED 6" TO 12" OF SEDIMENT. INSPECT EVERY 1 TO 2 WEEKS AND AFTER EVERY MAJOR RAINFALL EVENT.

SILT SACK

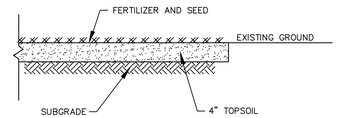
NOT TO SCALE



NOTES:
1. MATCH EXISTING PAVEMENT THICKNESS OR USE MINIMUM PAVEMENT THICKNESS SHOWN (WHICHEVER IS GREATER).
2. REFER TO APPROPRIATE TRENCH DETAIL FOR TRENCH LIMITS.

PERMANENT PAVEMENT REPAIR
(ARTERIAL ROAD)

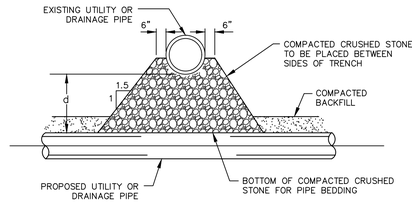
NOT TO SCALE



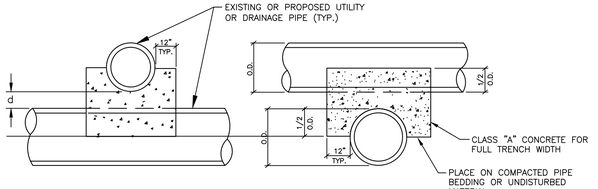
NOTES:
1. REFER TO CONTRACT SPECIFICATIONS FOR SEED MIXTURES.
2. PERMANENT GRASS SEED MIXES SHALL BE APPLIED FROM APRIL 1 THOUGH JUNE 15 OR AUGUST 15 THROUGH OCTOBER 1.
3. PERENNIAL RYEGRASS SHALL BE APPLIED AS TEMPORARY GRASS SEED FROM MARCH 15 THROUGH JULY 1 OR AUGUST 1 THROUGH OCTOBER 15.

RESTORATION OF LAWN AREAS

NOT TO SCALE



CRUSHED STONE SUPPORT WHEN 18" ≥ d >12"



CONCRETE PIPE CRADLE WHEN d ≤ 12"

NOTES:
1. d = DISTANCE BETWEEN UTILITY AND DRAINAGE PIPES.
2. SUPPORTS SHALL BE INSTALLED WHERE SPECIFIED ON THE PLANS AND WHERE DIRECTED BY THE ENGINEER.
3. CRUSHED STONE SUPPORTS SHALL BE INCLUDED IN THE COST OF THE PROPOSED UTILITY OR DRAINAGE PIPE AND CONCRETE PIPE CRADLES SHALL BE PAID FOR AS "MISCELLANEOUS CONCRETE".

TYPICAL UTILITY SUPPORTS

NOT TO SCALE

TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET – P.O. BOX 191
MANCHESTER, CT 06045–0191

LEGEND

WETLANDS BOUNDARY	LIGHT POLE
RETAINING WALL	CONIFEROUS TREE
GUIDE RAIL	DECIDUOUS TREE
STONE WALL	SANITARY MANHOLE
STOCKADE FENCE	DRAINAGE MANHOLE
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RAILROAD TRACKS	CURB STOP
SILT FENCE	WATER VALVE
CONCRETE MONUMENT	BUTTERFLY VALVE
GRANITE MONUMENT	BLOW OFF
IRON PIPE	SIGN
IRON ROD	DOUBLE POST SIGN
CONTROL POINT	MAIL BOX
DRILL HOLE	BOLLARD
UTILITY POLE	CONTROLLER CABINET
UTILITY POLE WITH LIGHT	GAS GATE
TRAFFIC SPAN POLE	TELEPHONE BOX
ELECTRIC BOX	CATV TUBE
WETLAND FLAG	

PROJECT NUMBER
2023099

FILENAME
2023099PLAN.DWG

NO.	DATE	FILE
–	08/07/25	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JD
RELEASED BY: JL

DRAWING SCALE
NTS

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
DOVER ROAD
HENDEE ROAD
EDISON ROAD
JARVIS ROAD
FULTON ROAD
WHITNEY ROAD

PROJECT TITLE
JARVIS ROAD NEIGHBORHOOD IMPROVEMENTS

SHEET TITLE
DETAILS

SHEET NUMBER
11 of 11

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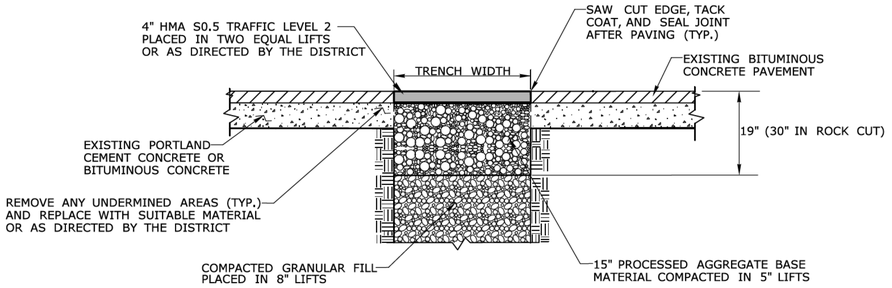
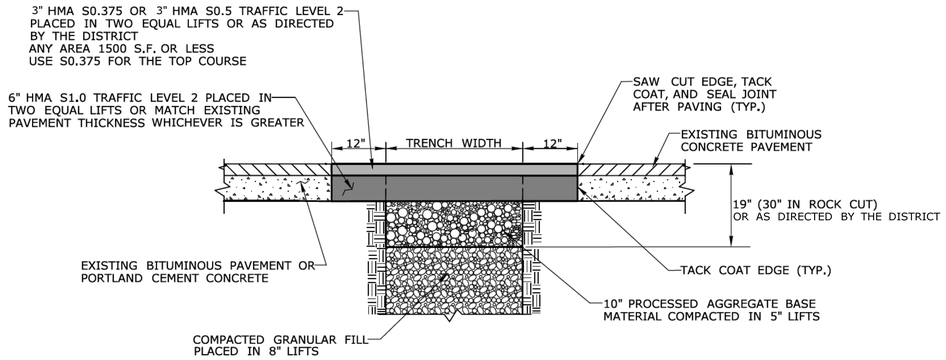
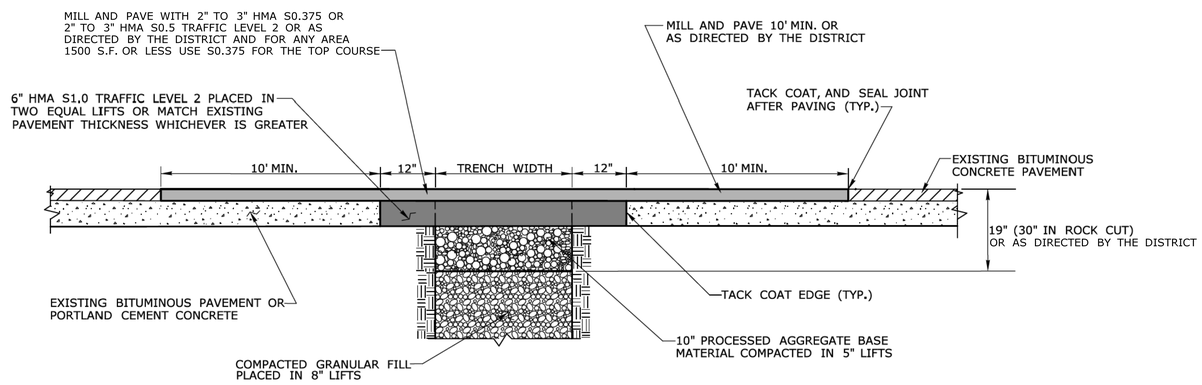


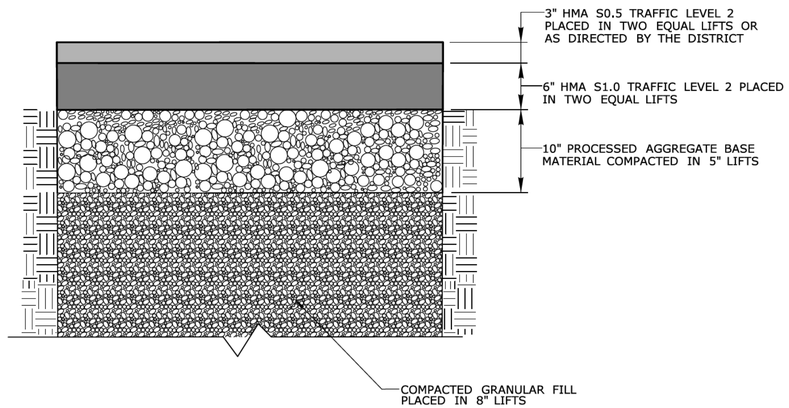
TEMPORARY PAVEMENT REPAIR FOR TRENCH THROUGH OVERLAID PORTLAND CEMENT CONCRETE OR BITUMINOUS CONCRETE PAVEMENT



PERMANENT PAVEMENT REPAIR WITHOUT MILLING - THROUGH PORTLAND CEMENT CONCRETE OR BITUMINOUS CONCRETE PAVEMENT



PERMANENT PAVEMENT REPAIR WITH MILLING



ROADWAY PROFILE

GENERAL NOTES:

1. LONGITUDINAL TRENCHING FOR JOINTED CONCRETE PAVEMENT:

   A. IF THE LONGITUDINAL TRENCH FALLS BETWEEN THE SLAB CENTERLINE AND THE EDGE OF SLAB, REMOVE CONCRETE AND BITUMINOUS CONCRETE PAVEMENT FROM THE TRENCH EDGE TO THE EDGE OF ROAD. IF THE LONGITUDINAL TRENCH FALLS BETWEEN THE LONGITUDINAL JOINT AND THE SLAB CENTERLINE, REMOVE THE ENTIRE CONCRETE SLAB AND BITUMINOUS CONCRETE PAVEMENT TO THE EDGE OF ROAD. IN EITHER CASE REBUILD WITH THE FOLLOWING:

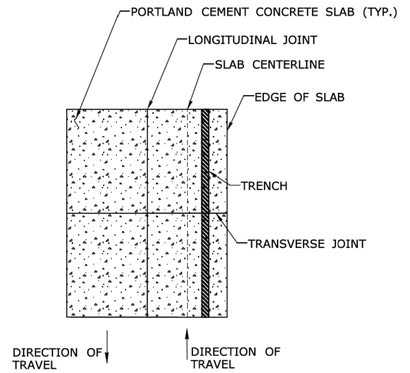
      a. PLACE HMA S1.0 TRAFFIC LEVEL 2 IN TWO EQUAL 4" - 5" LIFTS TO MATCH EXISTING CONCRETE PAVEMENT THICKNESS
      b. PLACE HMA S0.5 TRAFFIC LEVEL 2 IN 2" - 3" LIFTS TO MATCH EXISTING BITUMINOUS CONCRETE PAVEMENT THICKNESS, WITH THE FINAL LIFT BEING 2"

2. TRANSVERSE TRENCHING FOR JOINTED CONCRETE PAVEMENT:

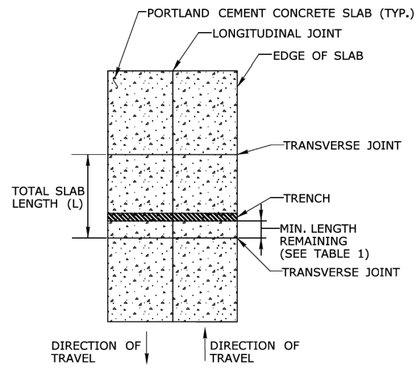
TABLE 1	
TOTAL SLAB LENGTH (L)	MIN. LENGTH REMAINING
40' OR LONGER	1/4 L
15' - 40'	10'
15' OR SHORTER	REBUILD TO NEAREST JOINT

   A. FOR TRANSVERSE TRENCHES, THE MINIMUM SLAB LENGTH AS SHOWN IN TABLE 1 SHALL BE LEFT IN PLACE TO THE NEAREST TRANSVERSE JOINT. IF THIS CRITERIA CANNOT BE MET, THE EXISTING SLAB AREA FROM THE TRENCH EDGE TO THE NEAREST TRANSVERSE JOINT SHALL BE REMOVED AND REBUILT AS FOLLOWS:

      a. PLACE HMA S1.0 TRAFFIC LEVEL 2 IN TWO EQUAL 4" - 5" LIFTS TO MATCH EXISTING CONCRETE PAVEMENT THICKNESS
      b. PLACE HMA S0.5 TRAFFIC LEVEL 2 IN 2" - 3" LIFTS TO MATCH EXISTING BITUMINOUS CONCRETE PAVEMENT THICKNESS, WITH THE FINAL LIFT BEING 2"

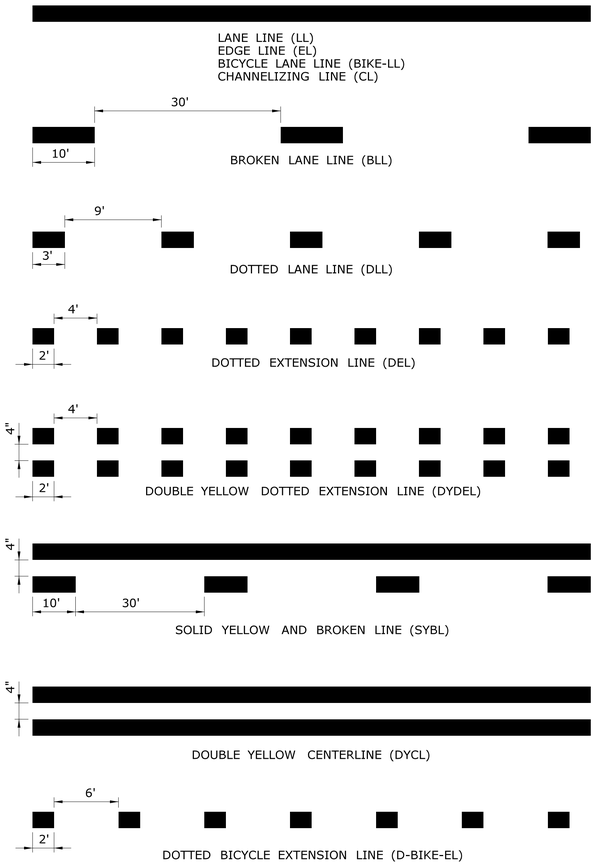


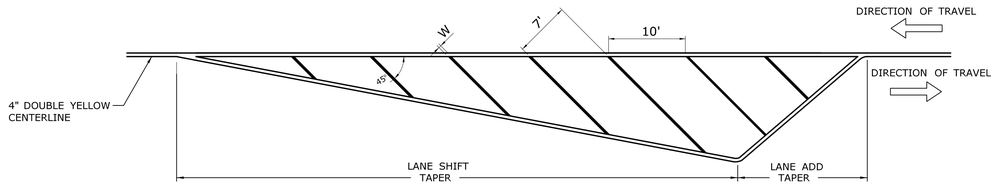
LONGITUDINAL TRENCHING FOR JOINTED CONCRETE PAVEMENT
(SEE NOTE 1)



TRANSVERSE TRENCHING FOR JOINTED CONCRETE PAVEMENT
(SEE NOTE 2)

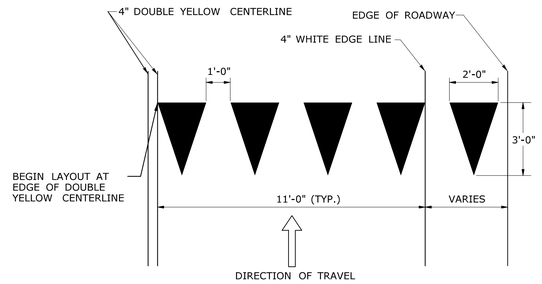
DRAFTER: MS CHECKED BY: EL NO SCALE	HIGHWAY OPERATIONS	OFFICE OF MAINTENANCE OPERATIONS SPECIAL SERVICES AND PLANNING	STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION	DRAWING TITLE: **ENCROACHMENT PERMIT - PAVEMENT REPAIR**



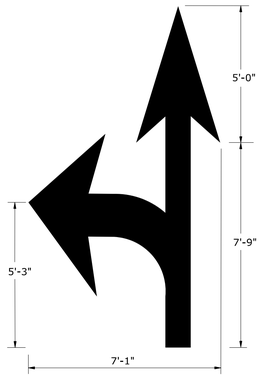


CROSS HATCHED ISLAND DETAIL
(YELLOW)

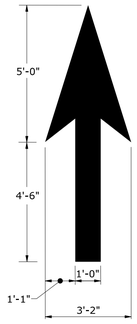
W IS TO BE 6" WHEN POSTED SPEED ≤ 45 MPH
W IS TO BE 12" WHEN POSTED SPEED > 45 MPH
CROSS HATCHED ISLANDS ARE TO BE INSTALLED WHERE CALLED FOR ON THE PLANS



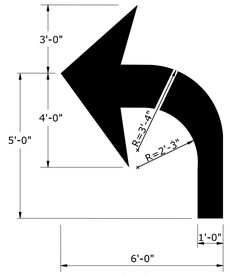
YIELD LINE DETAIL
(WHITE)



REFER TO ADJACENT VIEWS FOR DIMENSIONING
27.0 S.F.



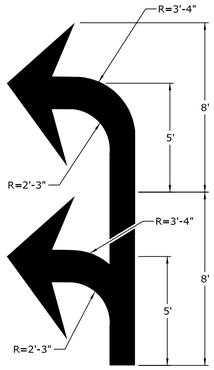
12.5 S.F.



15.5 S.F.

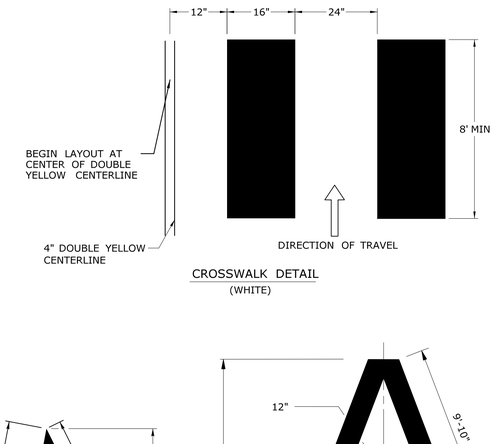
27.0 S.F.
LEFT & RIGHT
ARROW COMBO

38.5 S.F.
3 WAY ARROW
COMBO



35 S.F.
DOUBLE LEFT
ARROW

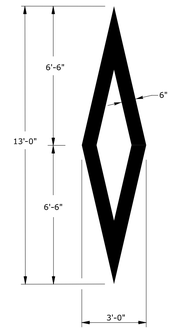
PAVEMENT ARROW DETAILS
(WHITE)
ARROWS SHALL BE CENTERED IN TRAVEL LANE



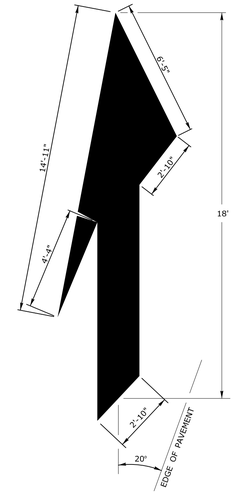
CROSSWALK DETAIL
(WHITE)

NOTES :

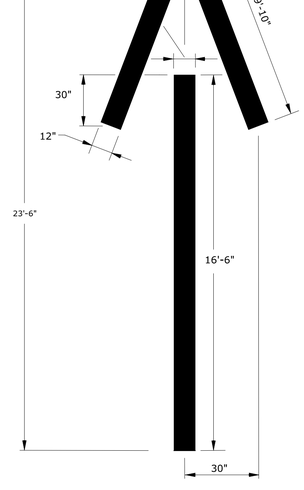
1. AREA OF PAVEMENT MARKINGS AS INDICATED IS APPROXIMATE.
2. RIGHT TURN PAVEMENT MARKING ARROWS ARE MIRROR IMAGE OF LEFT TURN PAVEMENT MARKING ARROWS.



WHITE PREFERENTIAL
LANE SYMBOL
13.0 S.F.



WHITE LANE
REDUCTION ARROW
41.8 S.F.



WHITE WRONG WAY
PAVEMENT ARROW
36.2 S.F.

REV.	DATE	REVISION DESCRIPTION
1	8-2018	REMOVED ROUNDABOUT MARKINGS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 8/10/2018

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

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	Mark F. Makuch, P.E. 2018.08.17 09:07:44-04'00'
APPROVED BY:	NAME/DATE/TIME:
	Mark F. Carlino, P.E. 2018.08.21 07:48:45-04'00'

CTDOT STANDARD SHEET

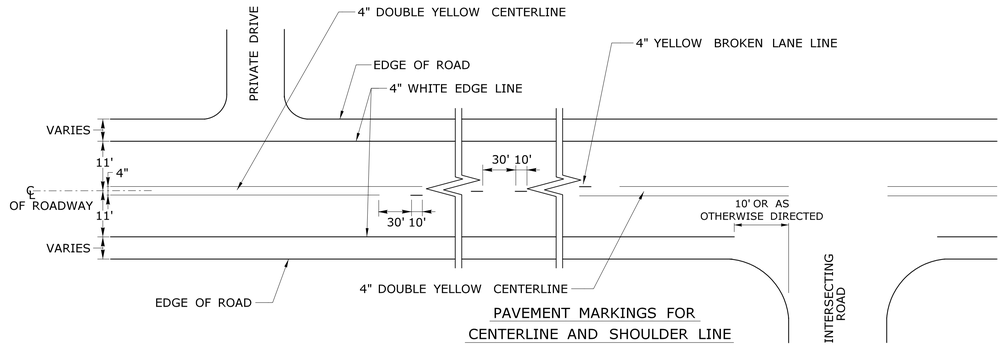
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

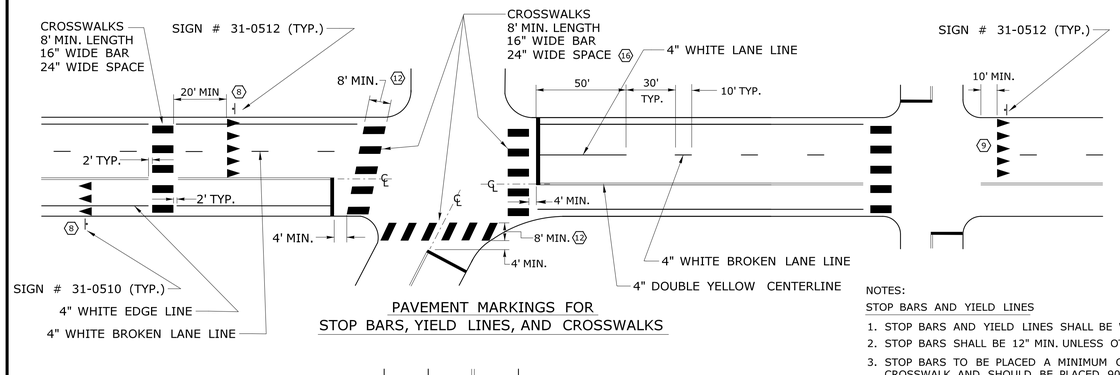
PAVEMENT MARKING LINES AND SYMBOLS

STANDARD SHEET NO.:

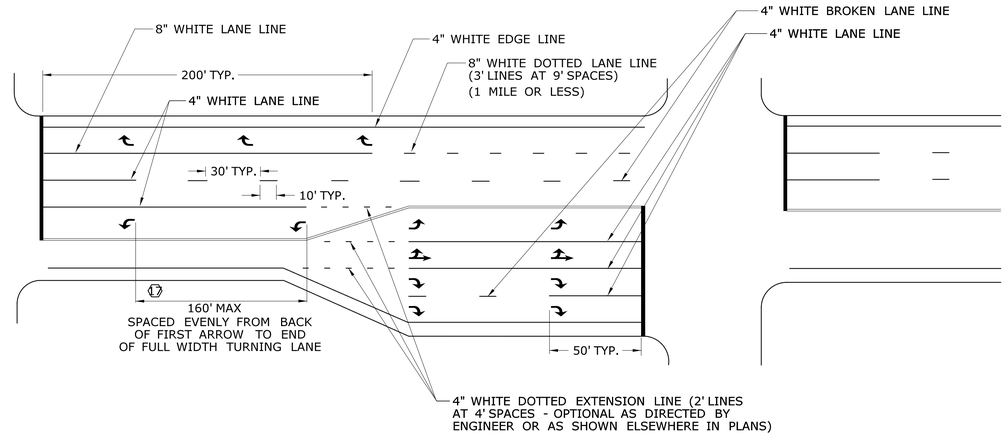
TR-1210_04



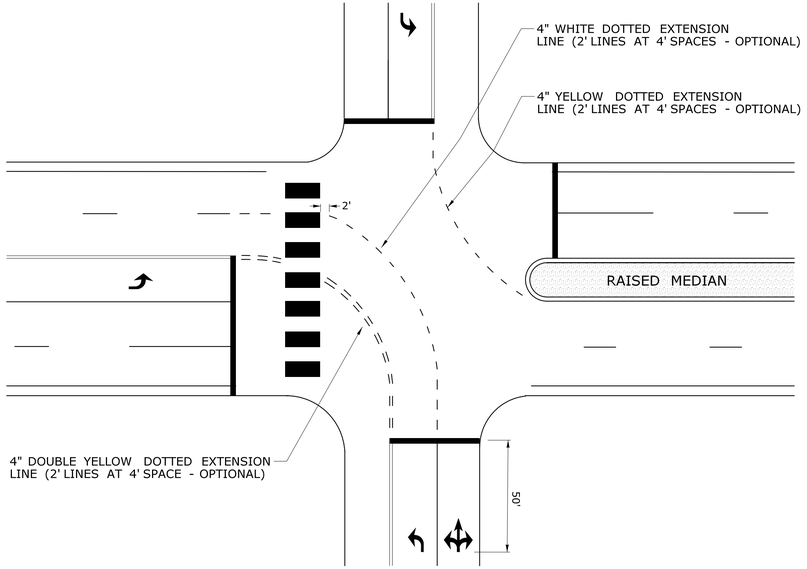
PAVEMENT MARKINGS FOR
CENTERLINE AND SHOULDER LINE



PAVEMENT MARKINGS FOR
STOP BARS, YIELD LINES, AND CROSSWALKS



PAVEMENT MARKINGS FOR TURNING LANES
AND ROAD LANE-DROP



TURNING EXTENSIONS

NOTES:

STOP BARS AND YIELD LINES

1. STOP BARS AND YIELD LINES SHALL BE WHITE.
2. STOP BARS SHALL BE 12" MIN. UNLESS OTHERWISE NOTED ON PLANS.
3. STOP BARS TO BE PLACED A MINIMUM OF 4' IN ADVANCE OF THE NEAREST EDGE OF CROSSWALK AND SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY.
4. IN THE ABSENCE OF A MARKED CROSSWALK THE STOP BAR SHOULD BE PLACED 90° TO THE CENTERLINE OF THE ROADWAY, AT THE DESIRED STOPPING POINT AT LEAST 5' AND NO MORE THAN 30' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.
5. THE STOP SIGN SHOULD BE PLACED IN LINE WITH THE STOP BAR. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
6. FOR STOP BARS AT RAMPS SEE DETAILS "R", "S", "T", & "U" AND NOTES ON TRAFFIC STANDARD SHEET TR-1210 07 "PAVEMENT MARKINGS FOR DIVIDED HIGHWAYS EXIT RAMPS".
7. FOR YIELD LINE INSTALLATIONS, ONLY FULL TRIANGLES ARE TO BE INSTALLED.

(8) MID-BLOCK CROSSWALKS ARE CROSSWALKS LOCATED MORE THAN 50 FEET FROM A SIGNALIZED OR UNSIGNALIZED INTERSECTION. YIELD LINES ASSOCIATED WITH MIDBLOCK CROSSWALKS SHALL BE INSTALLED AND SHOULD BE LOCATED 20 TO 50 FEET IN ADVANCE OF THE NEAREST CROSSWALK LINE OR AS DIRECTED BY THE ENGINEER.
WHERE A YIELD LINE EXISTS ON AN APPROACH TO A CROSSWALK, THE APPROAPRIATE "YIELD TO PEDESTRIANS" SIGN IS REQUIRED.

(9) FOR CROSSWALKS AT UNSIGNALIZED INTERSECTIONS WITH MINOR STREET STOP CONTROL, YIELD LINES SHALL BE INSTALLED ON MULTI-LANE APPROACHES, BUT NOT SINGLE LANE APPROACHES.

10. THE YIELD SIGN SHOULD BE PLACED IN LINE WITH A YIELD LINE. HOWEVER, IF THE YIELD SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO YIELD, THE YIELD LINE SHOULD BE PLACED AT THE YIELDING POINT.

CROSSWALKS

11. CROSSWALK MARKINGS SHALL BE WHITE.

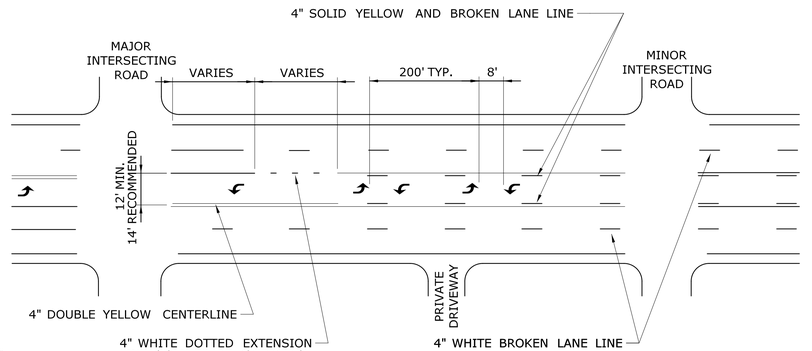
(12) AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO ℄ AND ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.

13. BARS SHOULD BE NO CLOSER THAN 1' FROM EDGE OF ROAD.
14. ONLY FULL LENGTH BARS ARE TO BE INSTALLED.
15. DECORATIVE CROSSWALKS SHALL BE BANDED FROM CURB TO CURB WITH A MINIMUM 12" WIDE WHITE TRANSVERSE LINE ALONG EACH EDGE.

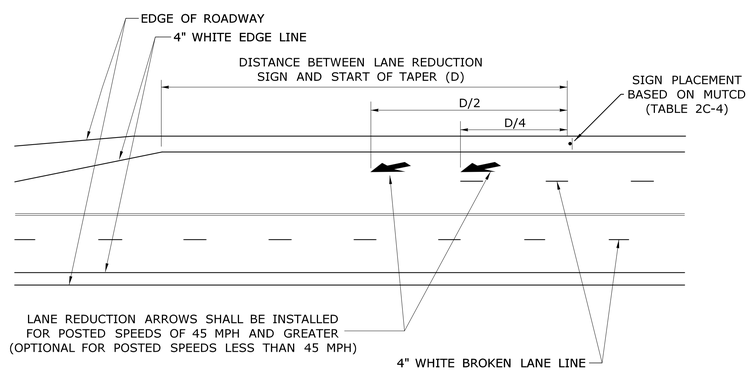
(16) 24" WIDE SPACE TO BE CENTERED ON YELLOW CENTERLINE.

PAVEMENT MARKINGS FOR TURNING LANES

(17) INSTALL AT LEAST TWO ARROWS PER LANE WHERE STORAGE LENGTH IS GREATER THAN 150 FEET.



TWO-WAY LEFT-TURN MARKINGS APPLICATIONS



LANE REDUCTION TRANSITIONS

REV.	DATE	REVISION DESCRIPTION
1	8-2018	REVISED YIELD LINE SIGNAGE AND NOTES.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 8/10/2018

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

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APPROVED BY:	NAME/DATE/TIME:
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CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:

PAVEMENT MARKINGS FOR NON FREEWAYS

STANDARD SHEET NO.:

TR-1210_08