

~~2019~~2024-~~2024~~2029

TOWN OF MANCHESTER

SIDEWALK AND CURB PLAN



Prepared by:

Manchester Planning Department
Manchester Public Works Department

Adopted ~~April 1, 2019~~TBD
Planning and Zoning Commission

SIDEWALK & CURB PLAN

Purpose

In accordance with Town ordinance § 279-31, which requires the installation of sidewalks and curbs by property owners simultaneous with new building construction, the purpose of this plan is:

- To set forth the policies for determining where sidewalks and curbs should be installed along public streets;
- To recommend priority locations for the extension of sidewalks; and
- To enumerate examples of what constitutes good cause for allowing or requiring payment in lieu of sidewalk installation.

Introduction

Providing paths for pedestrians has always been fundamental to community building, and while the need for and function of sidewalks has changed, it has not disappeared. The purpose of sidewalks is to provide a safe location for people to walk separated from motorized or mechanized vehicles. Sidewalks are an elemental form of transit, connecting people to public transit, schools, work, shopping, services, and cultural or recreational facilities and activities. They provide a space for spontaneous social interaction. They are increasingly used as a recreation and health amenity in themselves for walkers and joggers. For families with young children sidewalks provide a safe dedicated space for youngsters to learn to ride a bike or rollerblade.

For all of these reasons, Manchester has long required individuals and businesses developing land in Manchester to construct sidewalks. The goal is to provide pedestrian connections within neighborhoods, connections between neighborhoods, and connections from homes to services, facilities, and amenities in the community.

The Town's Sidewalk Plan was first adopted in 1980. Since that time there have been ~~at least~~ ~~two~~ several periods of significant new industrial, commercial and residential development in Manchester. ~~Because As~~ development ~~has extend~~ sed to previously undeveloped areas ~~and away from beyond~~ the Town center, the Planning and Zoning Commission periodically review ~~sed~~ the Sidewalk Plan considering current conditions and anticipated future trends. Our sidewalk and curb policies, and the location map that ~~will serve~~ s as a guide for the implementation of this Plan, have been revised to reflect these ~~new-current realities~~ condition. The Plan articulates policies for where new sidewalks should be installed; effectively and sensibly deals with new installations in developed areas; sets guidelines on how and whether to decide to remove

sidewalks in certain areas, identifies priorities for filling in gaps and/or extending the existing system; and provides guidance on what types of curbs should be installed in various locations or conditions.

Maps

The following maps are included with this Plan:

- **Sidewalk Location Map** – a map of all existing and anticipated future sidewalks along public streets, identified by material type.
- **Roadway Classification Map** – the classification of all public streets by type, to be used in accordance with the policies outlined below for locating new sidewalks.

Policy for New Sidewalk Installation

As a general policy, the Sidewalk Plan calls for concrete sidewalks on ~~all~~most streets in the Town of Manchester. The amount and location of sidewalks will vary depending on the type of street and its function. Table One describes the minimum standards and locations for sidewalks. Roadway classifications are shown on the Roadway Classification Map, Town of Manchester Sidewalk Plan, ~~dated~~revised July 2009(date TBD).¹

On all new local streets, sidewalks will be constructed as required in Table One unless the judgment is made by the Planning and Zoning Commission, with the recommendation of the Public Works Department (PWD), that payment in lieu of installation is warranted. On all existing local² streets, sidewalks will be required as identified on the Sidewalk Location Map, unless the judgment is made by the Planning and Zoning Commission, with the recommendation of PWD, that payment in lieu of installation is warranted.

Table One

Minimum Standards for Sidewalks					
Roadway Classification	Sidewalk Width (feet)	Concrete	Bituminous or Alternative	One Side	Two Sides
Arterial Street	5	x			x
Collector Street	5	x			x
Local Street	5	x		x	
Rural/Scenic Road	*6-8		x	x	

* based on site conditions

¹ Rev. 1/21/09

² Rev. 1/21/09

Regardless of the general policy and standards recommended in this Plan and contained in Table One above, the location of sidewalks on existing streets shall be based on the Sidewalk Location Map which is a part of this Plan. Sidewalks will be required on all street segments identified as Proposed Sidewalk Extensions, and repairs, if necessary as required by the Town Sidewalk Ordinance (see Appendix A), will be required on all segments identified as existing sidewalk. The Plan may require that sidewalks be built on both sides of an existing local street if it is deemed necessary for pedestrian safety given the proximity to schools, the housing density of the neighborhood, and other factors.

When required on only one side of the street, sidewalks shall be installed on the north and east sides of the roadway to facilitate snow melt and ease of maintenance. While this general standard should always apply, there may be circumstances where the location, width, or material requirements may be waived. Conditions that may warrant a deviation from these standards include the following:

- Infill development in neighborhoods or on streets where the sidewalks should match the dominant pattern on the street or in the neighborhood.
- Physical features of the area including the available right-of-way, grades, rocks/ledge, specimen trees or other important natural features which should be preserved, etc. In these cases the Director of Public Works may recommend, and the Planning and Zoning Commission may approve, alternate locations.

All sidewalks shall be built according to the design and construction standards contained in the Town of Manchester Public Improvement Standards, as amended.

Critical Path Extensions

The Sidewalk Plan has identified the desirable locations for sidewalks throughout the Town of Manchester. Keeping in mind this Plan will be updated every five years, the Plan identifies selected streets as priorities for the extensions of new sidewalks. Criteria for determining these critical path connections include the following:

- The desire to provide safe pedestrian connections to schools, shopping, or transit facilities, with consideration given to the school location, residential density and nature of the streets and traffic.
- The desire to provide safe pedestrian walkways on arterial or collector streets where such pedestrian systems are not now available, in particular highly populated or highly traveled areas.
- The desire to close gaps in sidewalk systems in areas of high pedestrian traffic.

The priority streets identified as part of this Plan are:^{3 4}

- ~~Broad Street from Middle Turnpike West to Hilliard Street~~
- Tolland Turnpike
- Keeney Street from Bush Hill Road north
- Parker Street from East Center Street to Mather Street
- Sheldon Road
- Woodland Street
- Woodbridge Street from Parker Street to Weaver Road
- East Center Street from Goodwin Street to Middle Turnpike East
- Charter Oak Street
- Wyllys Street/Porter Street
- Autumn Street
- Bush Hill Road
- Hilliard Street from Duval Street to Fleming Road

Although these are priority streets for extensions, such extensions are dependent upon the availability of funds and may not be included-completed during the 20192024-2024-2029 planning period. Likewise, other streets may see their sidewalks extended if funding opportunities arise, or if circumstances arise during the planning period that warrant sidewalk extensions on non-priority streets.

Removal of Sidewalks

~~In some circumstances in older residential neighborhoods where there are sidewalks on both sides of the street, it may be desirable to remove sidewalks to meet the current standard of sidewalks on only one side of the street. Removing walks on one side of the street would bring older neighborhoods in line with current practice for local streets. The removal of sidewalks within narrow rights-of-way provides opportunities to create greater separation of pedestrian and vehicular traffic. Selective removal would allow more neighborhood sidewalks to be repaired or replaced over the long term, and the Town's long-term maintenance and liability costs would be reduced. The removal of sidewalks is not generally recommended; however, the Town recognizes that unique circumstances may arise in which removals could be considered. Such instances will be reviewed on a case by case basis.~~

Sidewalk removal projects will be pursued only after an analysis of neighborhood conditions has been completed and public meetings with neighborhood residents have been held. Conditions which may warrant the removal of sidewalks include the following:

- The amount of right-of-way available in the existing public street area.

³ Rev. 1/21/09

Rev.7/6/09

Rev.02/13/14

⁴ Rev. 04/01/19

- The density of housing in the neighborhood. Generally the lower the housing density and the smaller the resident population, the more unlikely sidewalks will be necessary on both sides of the street.
- On streets with low traffic volumes and/or which are not major pedestrian corridors, sidewalks will less likely be necessary on both sides of the street.
- Neighborhoods whose distance from commercial or service uses would generate less pedestrian traffic may not require sidewalks on both sides of the street.
- Neighborhoods with wide rights-of-way, exclusively residential areas and neighborhoods not in proximity to non-residential uses.

Granite Curbing Installation

- Installation of granite curbs shall be required on all new local streets and extensions of existing local streets.
- Granite curbs shall be required on all collector and arterial streets as shown on a Town of Manchester Roadway Classification Map.
- When individual lot development occurs within a developed local street, the type of curb required should be consistent with the predominant material currently existing on that street, as determined by ~~PWD~~DPW.
- When existing roadways and sidewalks are reconstructed on local streets, curbs should be replaced with the predominant curb type that exists within the neighborhood, as determined by PWD.
- On rural/scenic roads, the determination whether to install granite curbs or alternate materials will be based upon such factors as the street grade, stormwater management considerations, and natural features and topography.

Rural/Scenic Roads

Although Manchester is a largely developed community, there are some roads that are, and should remain, rural and scenic in character. These are roads in outlying areas that are mostly identified as ~~natural or rural/agricultural~~conservation sectors on the ~~proposed character map~~Conservation & Growth Map in the Town's Plan of Conservation and Development, ManchesterNEXT. Future development in these areas is anticipated to be at low densities that would not conflict with rural or scenic roads. In these areas a pedestrian path should be provided, but a bituminous or stone dust pedestrian path may be more appropriate than granite curbs and concrete sidewalks.

Good Cause for Payment in Lieu of Installation

~~The~~ This Plan identifies desirable locations for sidewalks and curbs⁵ throughout the community. The Plan recognizes that, depending on the amount of Town financial resources available, and the development pattern in various areas, it may not be prudent or practical to install sidewalks and curbs⁶ in all new developments approved during the current five--year planning period. Where the Plan calls for sidewalk and curb installation, but where there is good cause not to install them at the time of the development, payment in lieu of installation shall be required in accordance with the Town Sidewalk Ordinance (see Appendix A).

Criteria-Situations in which requirement of payment in lieu of installation may~~to~~ be considered by the Planning and Zoning Commission following a recommendation by the Director of Public Works when requiring payment in lieu of installation would include, but are not ~~be~~-limited to, the following:

- There are no other sidewalks or curbs within a reasonable distance of the area where the sidewalks would be required.
- There are no sidewalks or curbs in the area or on the street where new walks would be otherwise required.
- There is no project funded by the Town that would fill in the gaps that exist in the sidewalk and/or curb system on the street where the development is proposed.
- There are plans in the near term for major street reconstruction which would provide opportunities to fill gaps in the sidewalk and/or curb system, but it would be better to construct the required sidewalks as part of the larger reconstruction project.
- When a sidewalk and or granite curbs are to be located within the right of way of a State road and the State denies a permit for the installation⁷.

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⁵ Rev. 1/21/09

⁶ Rev. 7/06/09

⁷ Rev. 7/06/09



SIDEWALK LOCATION MAP

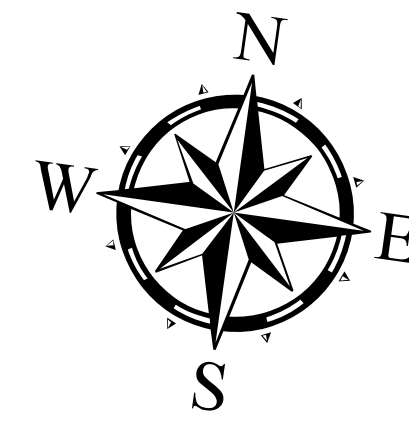
TOWN OF MANCHESTER SIDEWALK & CURB PLAN

Approved: April 1, 2019

UPDATE AFTER ADOPTION

Effective: April 12, 2019

Date Plotted: 09-Mar-2021

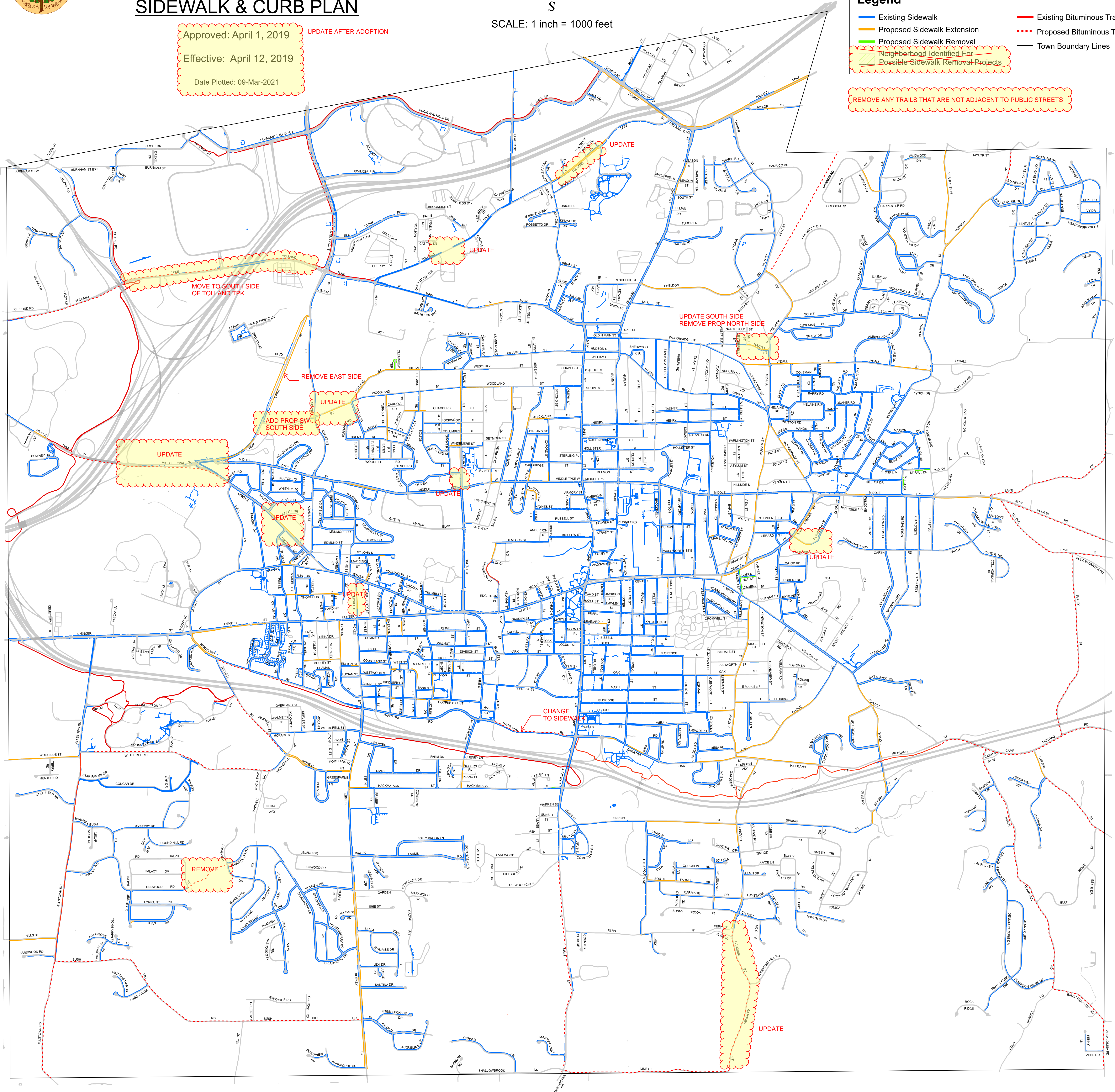


SCALE: 1 inch = 1000 feet

Legend

- Existing Sidewalk
- Proposed Sidewalk Extension
- Proposed Sidewalk Removal
- Neighborhood Identified For Possible Sidewalk Removal Projects
- Existing Bituminous Trail/Bike Lane
- Proposed Bituminous Trail/Bike Lane
- Town Boundary Lines

REMOVE ANY TRAILS THAT ARE NOT ADJACENT TO PUBLIC STREETS



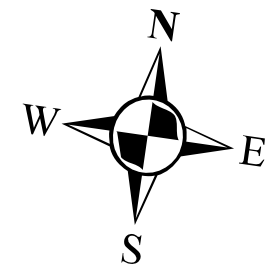


ROADWAY CLASSIFICATION MAP

TOWN OF MANCHESTER SIDEWALK & CURB PLAN

PRINTED: November 2016 1 inch = 800 feet

ADOPTED: APRIL 7, 2014
EFFECTIVE: APRIL 28, 2014
REVISED: JANUARY 19, 2016



LEGEND

- | | |
|-----------|--------------------|
| ARTERIAL | RURAL-SCENIC ROADS |
| COLLECTOR | ROAD RIGHT-OF-WAY |
| LOCAL | BRIDGES |
| OTHER | TOWN BOUNDARY |
- STATE ROADS
- | |
|-----------------|
| STATE HIGHWAY |
| STATE ARTERIAL |
| STATE COLLECTOR |

