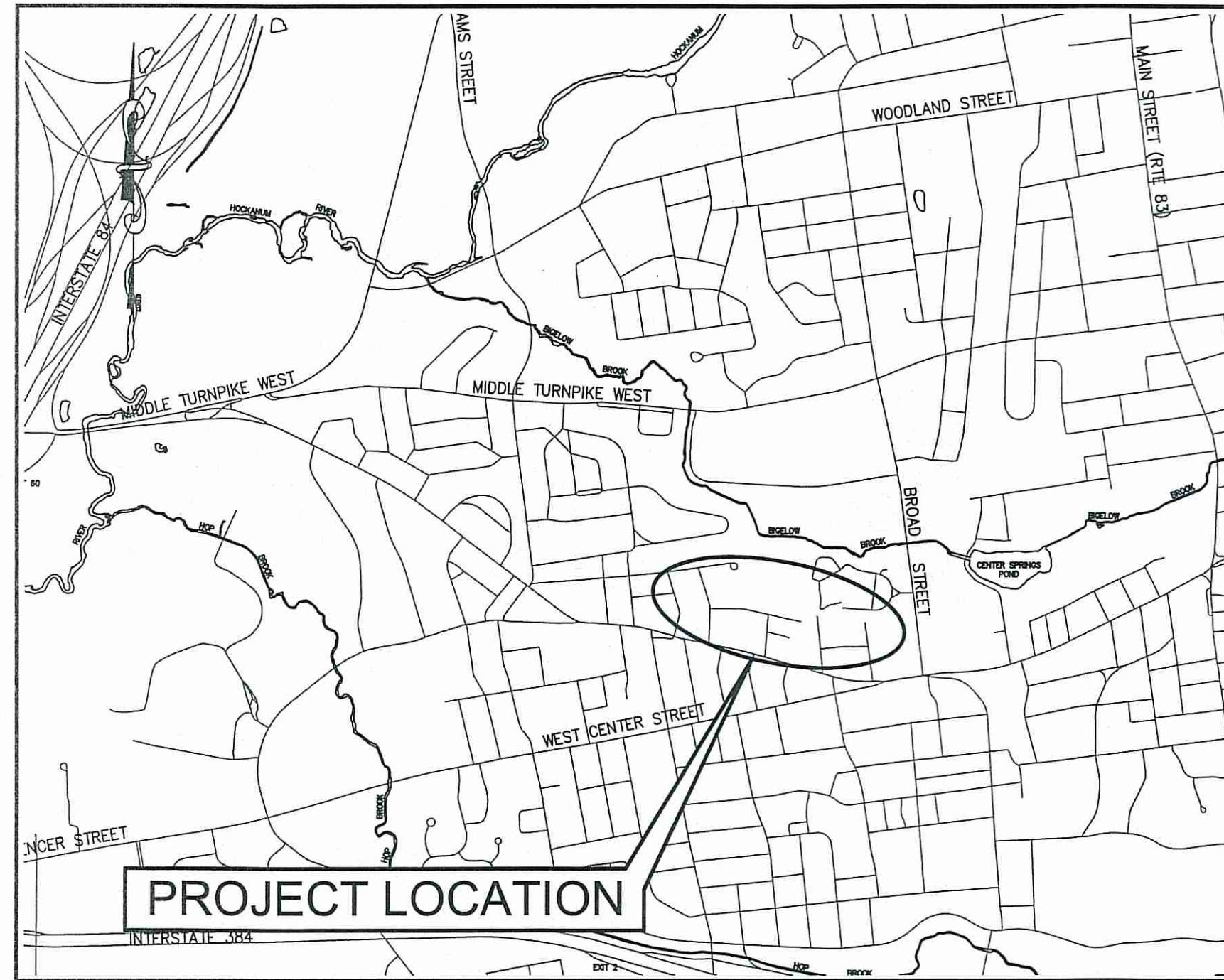


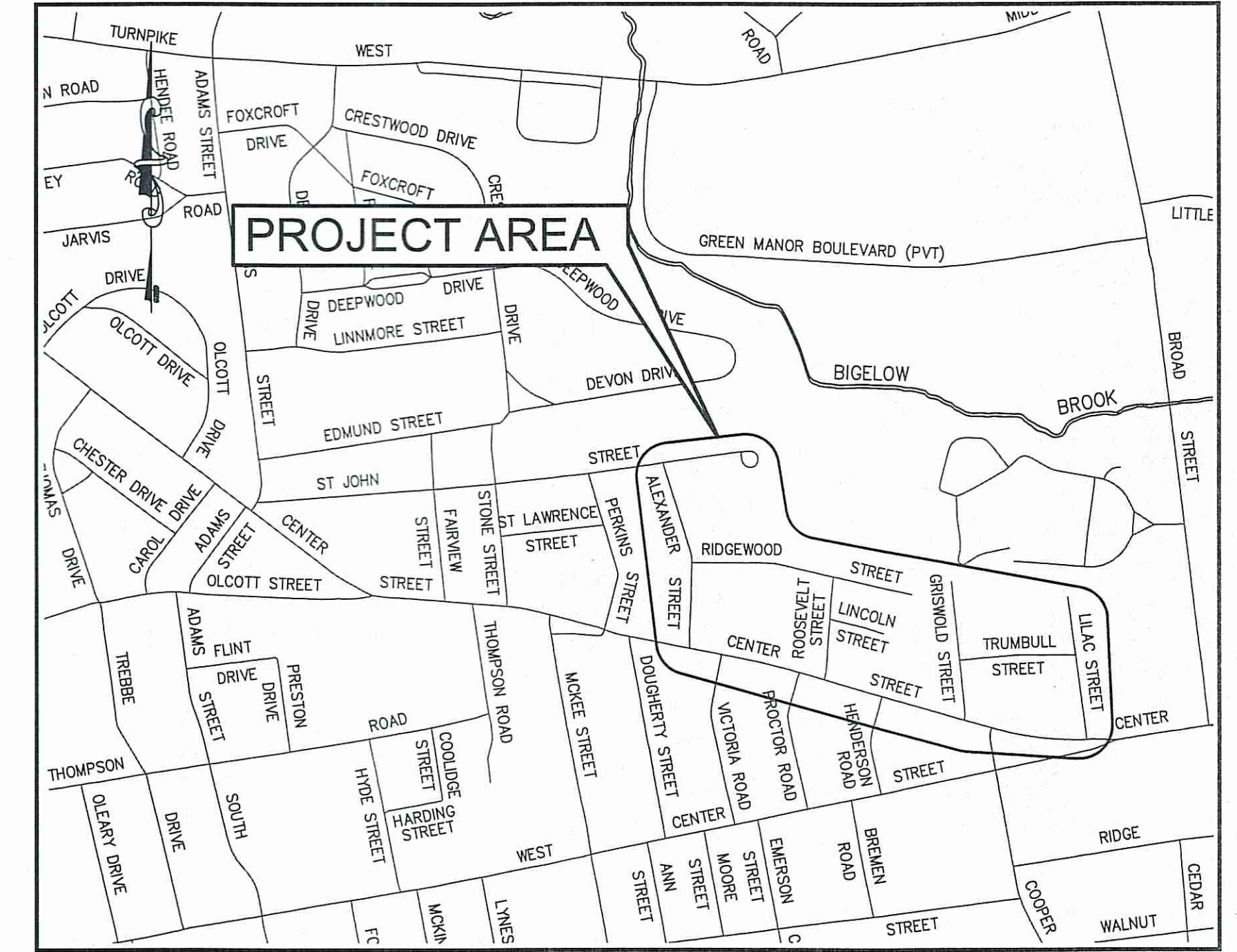
TOWN OF MANCHESTER

PUBLIC WORKS DEPARTMENT

ENGINEERING DIVISION



LOCATION MAP
1" = 1500'



SITE MAP
1" = 600'

RIDGEWOOD STREET-GRISWOLD STREET

AREA IMPROVEMENTS

JUNE 2022

DESIGN STANDARD : TOWN OF MANCHESTER PUBLIC IMPROVEMENT STANDARDS, EFFECTIVE DATE OCTOBER 31, 2020, AS AMENDED

DATUMS : HORIZONTAL DATUM: TOWN OF MANCHESTER CONTROL NETWORK (NAD83 AS ESTABLISHED IN 1998)
VERTICAL DATUM: TOWN OF MANCHESTER CONTROL NETWORK (NAVD88 USING GEOID 96)

STANDARD SPECIFICATIONS : SEE CONTRACT DOCUMENTS

DESIGN SCALES : PLAN: 1" = 20'
OTHER SCALES AS NOTED

LIST OF DRAWINGS	
SHEET NO.	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES & TYPICAL SECTION
3	ST. JOHN STREET - PLAN & PROFILE
4	ALEXANDER STREET - PLAN & PROFILE (STA 0+00 TO 4+00)
5	ALEXANDER STREET - PLAN & PROFILE (STA 4+00 TO 8+50)
6	RIDGEWOOD STREET - PLAN & PROFILE (STA 0+00 TO 4+50)
7	RIDGEWOOD STREET - PLAN & PROFILE (STA 4+50 TO 10+16)
8	ROOSEVELT STREET - PLAN & PROFILE
9	LINCOLN STREET - PLAN & PROFILE
10	GRISWOLD STREET - PLAN & PROFILE (STA 0+00 TO 4+75)
11	GRISWOLD STREET - PLAN & PROFILE (STA 4+75 TO 6+25)
12	TRUMBULL STREET - PLAN & PROFILE
13	LILAC STREET - PLAN & PROFILE (STA 0+00 TO 4+75)
14	LILAC STREET - PLAN & PROFILE (STA 4+75 TO 6+35)
15-20	CONSTRUCTION DETAILS

DESIGNED BY:
TOWN OF MANCHESTER
ENGINEERING DIVISION

Bernard Kalansuriya
BERNARD KALANSURIYA
DESIGN ENGINEER
P.E. NO. 22899

APPROVED BY:

John E. Dibiase
JOHN E. DIBIASI
ASSISTANT TOWN ENGINEER

GENERAL NOTES:

- ALL MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "MANCHESTER PUBLIC IMPROVEMENT STANDARDS", EFFECTIVE OCTOBER 31, 2020, AS AMENDED AND THE STATE OF CONN. DEPT. OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, FORM 818, DATED 2020, INCLUDING ANY SUPPLEMENTS.
- ALL ELEVATIONS ARE BASED ON THE NORTH AMERICAN VERTICAL DATUM OF 1988.
- IMPLEMENTING WORKER SAFETY AND HEALTH PROTOCOLS THAT ADDRESS COMPLIANCE WITH ALL RULES, LAWS AND REGULATIONS REGARDING SAFETY AND RISK OF EXPOSURE TO PHYSICAL AND CHEMICAL HAZARDS IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ALL EMPLOYEES OF THE CONTRACTOR AND SUBCONTRACTORS ARE TO WEAR REFLECTIVE VESTS AND HARD HATS AT ALL TIMES WHEN ON THE PROJECT SITE.
- A PRECONSTRUCTION MEETING WITH TOWN STAFF IS REQUIRED PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITY.
- THE CONTRACTOR SHALL PHASE CONSTRUCTION OPERATIONS TO MINIMIZE THE SIZE OF DISTURBED AREAS AND PERIOD OF TIME THESE AREAS ARE LEFT UNSTABILIZED AND SUBJECT TO EROSION. THIS INCLUDES, BUT IS NOT LIMITED TO, INSTALLATION OF DRAINAGE SYSTEMS DURING THE EARLY STAGES OF CONSTRUCTION AND LIMITING LENGTHS OF RECLAMATION AND FULL-DEPTH ROAD RECONSTRUCTION AREAS TO ALSO LESSEN IMPACTS TO VEHICLE AND PEDESTRIAN TRAVEL THROUGH THE PROJECT AREA.
- THE CONTRACTOR SHALL TAKE CARE NOT TO DISTURB MONUMENTATION THAT MAY BE PRESENT BENEATH OR IN CLOSE PROXIMITY TO CONCRETE SIDEWALKS OR ADJACENT TO HYDRANTS THROUGHOUT THE PROJECT AREA (PARTICULARLY NEAR INTERSECTIONS). IMMEDIATELY NOTIFY THE ENGINEER IF SUCH MONUMENTATION IS ENCOUNTERED.
- THE CONTRACTOR IS RESPONSIBLE TO OBTAIN ALL REQUIRED PERMITS AND PAY ASSOCIATED FEES PRIOR TO ANY CONSTRUCTION ACTIVITY. THE CONTRACTOR MUST OBTAIN AN ENCROACHMENT PERMIT FROM THE STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, PRIOR TO BEGINNING WORK WITHIN THE CENTER STREET (ROUTE 6&44) RIGHT-OF-WAY AND SHALL BE RESPONSIBLE FOR ALL ASSOCIATED FEES AND REQUIREMENTS.
- THE CONTRACTOR SHALL CONFINE ALL OPERATIONS AND ACTIVITIES FOR CONSTRUCTION PURPOSES WITHIN THE STREET LINE AND LIMITS OF UTILITY EASEMENTS UNLESS SHOWN OTHERWISE ON THE PLANS.
- THE CONTRACTOR SHALL RESTORE TWO LANES OF TRAFFIC ON ALL LOCAL STREETS AT THE END OF EACH WORK DAY. IT IS ANTICIPATED THAT THE ROADS WILL REMAIN OPEN AT ALL TIMES WITH ALTERNATING ONE LANE TRAFFIC DURING CONSTRUCTION.
- THE CONTRACTOR SHALL COMMIT SUFFICIENT RESOURCES TO THE PROJECT TO ENSURE THE PROJECT IS COMPLETED WITHIN THE ALLOTTED CONTRACT TIME. ONCE MOBILIZED, THE CONTRACTOR SHALL WORK CONTINUOUSLY ON THE PROJECT UNTIL COMPLETION. ANY UNAUTHORIZED VACATING OF THE JOBSITE IS SUBJECT TO PENALTIES DESCRIBED UNDER THE "LIQUIDATED DAMAGES" SECTION OF THE CONTRACT SPECIFICATIONS.
- THE CONTRACTOR SHALL NOT STORE CONSTRUCTION EQUIPMENT OR MATERIALS WITHIN THE PUBLIC RIGHT-OF-WAY.
- CONSTRUCTION ENTRANCES ARE NOT SHOWN ON THE PLAN; HOWEVER, THEY SHALL BE INSTALLED WHERE DIRECTED BY THE ENGINEER DURING CONSTRUCTION FOR EGRESS FROM TEMPORARY STOCKPILE AREAS. THE PROPOSED LOCATION OF STOCKPILE AREAS SHALL BE IDENTIFIED BY THE CONTRACTOR.
- NO WORK SHALL COMMENCE UNTIL ALL CONSTRUCTION AREA SIGNS ARE IN PLACE.
- THE CONTRACTOR SHALL PROVIDE TEMPORARY ACCESS TO ALL DRIVEWAYS AT ALL TIMES.
- ALL GRASSED AREAS DISTURBED BY THE CONTRACTOR SHALL BE REPLACED WITH TOPSOIL, FERTILIZED AND SEEDED AS PER THE SPECIFICATIONS. CONTRACTOR SHALL MAKE ALL EFFORTS TO MINIMIZE THE LIMITS OF DISTURBANCE AND ASSOCIATED RESTORATION THAT IS REQUIRED.
- ANY DRIVEWAYS, SIDEWALKS, CURB AND LAWN AREAS LOCATED ON PRIVATE PROPERTY OR WITHIN THE RIGHT-OF-WAY THAT ARE IMPACTED DURING CONSTRUCTION SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS AS IDENTIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. THE REQUIRED LIMITS OF SUCH RESTORATION SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD. RESTORATION ON PRIVATE PROPERTY SHALL BE COMPLETED AS PROMPTLY AS PRACTICAL WITHIN THIRTY (30) CALENDAR DAYS OF COMPLETING WORK ON THE PROPERTY.
- PROTECTION, REMOVAL AND REPLACEMENT OF EXISTING FEATURES SUCH AS, BUT NOT LIMITED TO, WALLS, FENCES, TREES, SHRUBS AND PLANTS FOR INSTALLATION OF SEWER LATERALS, WATER SERVICES, SEGMENTAL BLOCK WALLS AND GENERAL SITE WORK WILL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCLUDED IN THE GENERAL COST OF THE CONTRACT. CONTRACTOR SHALL TAKE CARE TO PROTECT EXISTING FEATURES THAT ARE TO BE REINSTALLED. ALL OTHER FEATURES IDENTIFIED TO BE REPLACED ON THE PLANS SHALL BE PAID FOR UNDER THE ASSOCIATED CONTRACT BID ITEM. THIS WORK REQUIRED ON PRIVATE PROPERTIES SHALL BE COMPLETED AS PROMPTLY AS PRACTICAL WITHIN THIRTY (30) CALENDAR DAYS OF COMPLETING WORK ON THE PROPERTY.
- ACTUAL LIMITS OF CURB INSTALLATION AND DRIVEWAY RECONSTRUCTION SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. THE CONTRACTOR SHALL AVOID IMPACTS TO EXISTING DRIVEWAYS WHEREVER POSSIBLE TO MINIMIZE REQUIRED RECONSTRUCTION LIMITS.
- ALL SEDIMENT CONTROL SYSTEMS SHALL MEET THE "GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" AS PREPARED BY THE CONNECTICUT COUNCIL ON SOIL AND WATER CONSERVATION, LATEST REVISION. THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION, MAINTENANCE AND REPAIR OF EROSION CONTROLS REQUIRED FOR THE PROJECT. ADDITIONAL EROSION CONTROLS SHALL BE INSTALLED BY THE CONTRACTOR FOR TEMPORARY STOCKPILING OF EXCAVATED MATERIAL AND WHERE DEEMED NECESSARY BY THE ENGINEER. EROSION CONTROLS SHALL REMAIN IN PLACE AND BE MAINTAINED BY THE CONTRACTOR UNTIL THE SITE IS STABILIZED AND THE ENGINEER APPROVES THEIR REMOVAL.
- SILT SACKS SHALL BE INSTALLED IN ALL EXISTING AND PROPOSED CATCH BASINS WITHIN THE PROJECT AREA AND WHERE DIRECTED BY THE ENGINEER. SILT SACKS SHALL BE THE APPROPRIATE TYPE FOR CATCH BASINS WITH AND WITHOUT CURB INLETS.
- HORIZONTAL AND VERTICAL LOCATIONS OF PROPOSED WORK MAY BE ADJUSTED TO FIT EXISTING FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.
- EXISTING INFORMATION SHOWN ON THESE PLANS WAS OBTAINED FROM TOWN OF MANCHESTER GIS DATA AND LIMITED FIELD SURVEY; AND THEREFORE, IS CONSIDERED APPROXIMATE. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN INFORMATION SHOWN ON THE PLANS. LIMITS OF NEW WORK SHALL BE STAKED IN THE FIELD PRIOR TO CONSTRUCTION WHERE APPROPRIATE.

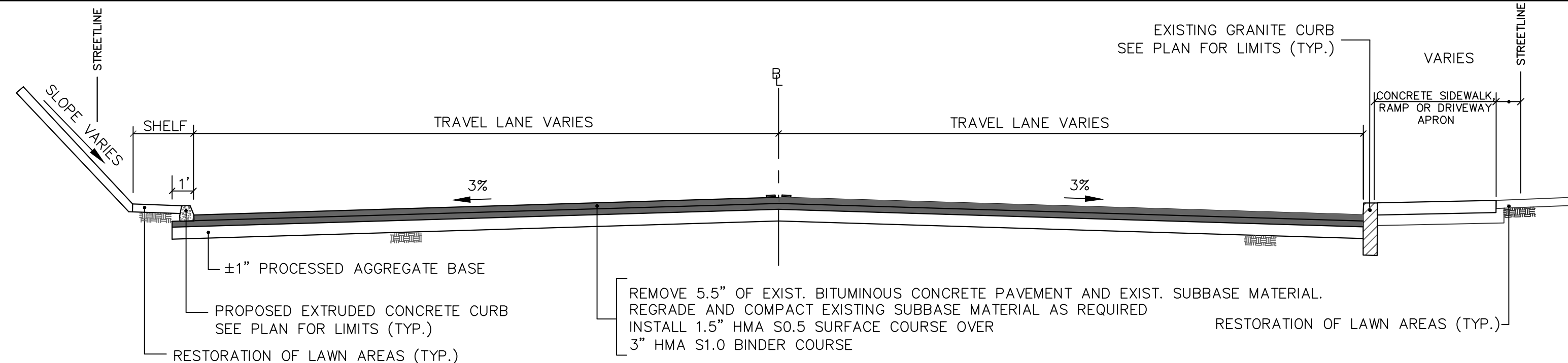
- THE EXISTENCE OF UTILITIES AND APPURTENANCES AS SHOWN ON THESE DRAWINGS ARE FOR REFERENCE ONLY. THE EXACT SIZE, LOCATION, TYPE, AND ELEVATION OF ALL UTILITIES WITHIN ALL WORK AREAS SHALL BE THOROUGHLY INVESTIGATED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING "CALL-BEFORE-YOU-DIG" AT 1-800-922-4455 AND MUST HAVE ALL UTILITIES MARKED ON THE GROUND PRIOR TO THE START OF CONSTRUCTION.
- THE QUANTITIES AS INDICATED IN THE CONTRACT DOCUMENTS ARE APPROXIMATE AND MAY NOT INDICATE THE ACTUAL QUANTITIES OF WORK REQUIRED. THE CONTRACTOR MUST VERIFY ALL QUANTITIES.
- SURPLUS EXCAVATED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR. THE CONTRACTOR SHALL DISPOSE OF SURPLUS EXCAVATED MATERIAL IN ACCORDANCE WITH STATE AND FEDERAL REGULATIONS.
- THE ROAD SHALL BE RECONSTRUCTED BY REMOVING THE EXISTING PAVEMENT SURFACE AND GRADING AND COMPACTING OF GRAVEL BASE MATERIAL FOR INSTALLATION OF HMA. LIMITED AREAS WHERE LOWERING OR WIDENING OF THE ROAD IS REQUIRED SHALL INCLUDE FULL-DEPTH RECONSTRUCTION WHERE IDENTIFIED ON THE PLANS OR WHERE DIRECTED BY THE ENGINEER DURING CONSTRUCTION. THESE AREAS SHALL BE EXCAVATED TO SUBGRADE, WHICH SHALL BE GRADED AND COMPACTED FOR INSTALLATION OF PROCESSED AGGREGATE BASE AND HMA IN ACCORDANCE WITH THE TYPICAL ROAD SECTION. EXCAVATION FOR BOX-OUT OF THE ROAD TO SUBGRADE ELEVATIONS IN THESE AREAS SHALL BE PAID AS "EARTH EXCAVATION". ALL HMA SHALL BE INSTALLED ON SUITABLE BASE MATERIAL.
- FINAL LIMITS OF FULL-DEPTH RECONSTRUCTION IN ROAD WIDENING AREAS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER. THESE AREAS SHALL EXTEND FROM A MIN. OF 2 FT INSIDE OF EXISTING PAVEMENT LIMITS TO 12" BEHIND THE PROPOSED FACE OF CURB OR EDGE OF ROAD.
- WHEN DIRECTED BY THE ENGINEER, TOPS, FRAMES AND COVERS FOR NEW STRUCTURES LOCATED WITHIN LIMITS OF ROAD RECONSTRUCTION SHALL BE TEMPORARILY SET AT THE BINDER COURSE ELEVATION AND RAISED TO THE FINAL COURSE ELEVATION IMMEDIATELY PRIOR TO PAVING. THERE WILL BE NO ADDITIONAL PAYMENT FOR RESET OF FRAMES AND COVERS TO FINAL ROAD ELEVATIONS.
- PROPOSED STRUCTURE FRAME ELEVATIONS IDENTIFIED ON THE PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL SET FRAME ELEVATIONS AS REQUIRED BASED ON EXISTING FEATURES AND GRADES IN THE VICINITY AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL SUBMIT A PLAN TO THE ENGINEER FOR PROPOSED METHODS TO DEWATER THE SITE (WHERE REQUIRED). SUCH PLAN SHALL INCLUDE THE INSTALLATION OF SILT BAGS AT PUMP DISCHARGES WITH SILT FENCE/HAYBALES FOR EROSION CONTROL. PROPOSED DISCHARGE LOCATIONS AND ALL EROSION CONTROLS SHALL BE REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.
- THE CONTRACTOR IS MADE AWARE THAT SOME WORK IS BEING COMPLETED IN BACKYARDS AND CLOSE TO EXISTING STRUCTURES; THEREFORE, NORMAL CONSTRUCTION PRACTICES MAY NOT BE FEASIBLE IN ALL INSTANCES. SMALLER EQUIPMENT AND HAND DIGGING MAY BE REQUIRED TO MINIMIZE DISTURBANCE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY HANDLING OF ALL STORMWATER RUNOFF DURING CONSTRUCTION. METHODS OF HANDLING RUNOFF SHALL BE APPROVED BY THE ENGINEER.
- AT THE END OF EACH WORKING DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING NEW DRAINAGE SYSTEMS TO EXISTING. ALL DRAINAGE SYSTEMS WITHIN THE CONSTRUCTION LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR.
- ALL NEW CATCH BASINS SHALL BE TYPE "C" WITH GRANITE CURB INLETS (0'-10" DEEP) UNLESS NOTED OTHERWISE. GRANITE CURB INLETS SHALL BE INSTALLED WHERE GRANITE CURB EXISTS AND CONCRETE CURB INLETS THAT MATCH CURB DIMENSIONS SHALL BE INSTALLED WHERE BITUMINOUS CONCRETE CURB OR CONCRETE CURB EXISTS. CATCH BASINS WITH 3-SIDED TOPS SHALL BE USED WHERE REQUIRED CURB REVEAL IS NOT ATTAINABLE AS SHOWN ON THE PLANS OR WHERE DIRECTED BY THE ENGINEER (TYPICALLY INSTALLED ADJACENT TO CONCRETE DRIVEWAY APRONS OR SIDEWALK RAMP).
- THE CONTRACTOR SHALL RESET GRANITE CURB IMMEDIATELY ADJACENT TO NEW CATCH BASIN TOPS AS DIRECTED BY THE ENGINEER WHEN TOP OF CURB ELEVATIONS ARE NOT UNIFORM. IF REQUIRED, THIS WORK WILL BE PAID FOR AS "RESET GRANITE STONE CURB" AND SHALL BE COMPLETED AS DIRECTED BY THE ENGINEER.
- ALL STREETS WITH DRAINAGE WILL BE RECONSTRUCTED TO PROVIDE APPROXIMATELY 4"-5" OF REVEAL ON EXISTING GRANITE CURB. THEREFORE, DRAINAGE STRUCTURE FRAME ELEVATIONS SHOULD BE SET 5.5" BELOW TOP OF CURB ELEVATIONS UNLESS DIRECTED OTHERWISE BY THE ENGINEER. TOP OF FRAME ELEVATIONS IDENTIFIED ON THE PLAN ARE APPROXIMATE.
- STREET CORNERS AND SIDEWALK RAMP SHALL BE CONSTRUCTED TO PROVIDE POSITIVE DRAINAGE TOWARDS THE NEAREST CATCH BASIN TO PREVENT PONDING OF STORMWATER. SIDEWALK RAMP SHALL BE INSTALLED TO MATCH PROPOSED GUTTER ELEVATIONS WHICH SHOULD PROVIDE 4"-5" OF REVEAL ON EXISTING GRANITE CURB ADJACENT TO RAMP.
- THE CONTRACTOR SHALL INSTALL UNDERDRAINS WHERE SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- LOCATION, SIZE AND PIPE MATERIALS SHOWN ON THE PLANS FOR EXISTING LATERAL CONNECTIONS (BLIND TIE-INS) TO THE EXISTING DRAINAGE SYSTEM ARE APPROXIMATE BASED ON VIDEO INSPECTIONS AND AVAILABLE MAPPING.
- ALL NEW CATCH BASINS SHALL HAVE 2 FT SUMP DEPTHS UNLESS OTHERWISE NOTED.
- ALL RCP INSTALLED AS PART OF THIS PROJECT SHALL BE CLASS IV WITH 2 FT MINIMUM COVER UNLESS NOTED OTHERWISE. CLASS V RCP SHALL BE INSTALLED IN LOCATIONS WHERE 1.5' TO 2' OF COVER IS ACHIEVABLE AS DIRECTED BY THE ENGINEER.
- DUCTILE IRON PIPE SHALL BE CLASS 52 AND INSTALLED IN LOCATIONS WHERE LESS THAN 1.5 FT OF COVER IS ACHIEVABLE AS DIRECTED BY THE ENGINEER. ALL PIPE SHALL HAVE 1 FT MINIMUM COVER.
- WHEREVER POSSIBLE, THE CONTRACTOR SHALL SCHEDULE DRAINAGE WORK SUCH THAT TRENCHES IN INTERSECTIONS CAN BE COMPLETELY EXCAVATED PRIOR TO PLACEMENT OF PIPE TO ENSURE UTILITY CONFLICTS DO NOT EXIST.
- EXISTING DRAINAGE PIPE SHALL BE REMOVED WHEN SUCH PIPE IS LOCATED WITHIN THE HORIZONTAL TRENCH LIMITS OF NEW PIPE TO BE INSTALLED IN THE SAME LOCATION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE LINEAR FOOT COST FOR THE ASSOCIATED PIPE INSTALLATION.
- FOR CONNECTIONS TO EXISTING DRAINAGE STRUCTURES, THE CONTRACTOR SHALL VERIFY THE EXISTING STRUCTURE INVERTS, NOTIFY THE ENGINEER IF A DISCREPANCY

- EXISTS, AND ADJUST THE PIPE SLOPES AS DIRECTED.
- ANY CORING OR OTHER MODIFICATIONS TO EXISTING STRUCTURES REQUIRED FOR CONNECTING NEW PIPES SHALL BE INCLUDED IN THE LINEAR FOOT COST FOR THE ASSOCIATED PIPE INSTALLATION. FOR THIS PROJECT, THE CONTRACTOR IS MADE AWARE THAT CONNECTIONS TO EXISTING CULVERTS, CATCH BASINS AND NON-STANDARD STRUCTURES ARE ANTICIPATED.
- THE CONTRACTOR SHALL NOT ABANDON ANY EXISTING DRAINAGE PIPES UNLESS CALLED FOR ON THE PLANS OR DIRECTED BY THE ENGINEER. PIPES THAT ARE TO BE ABANDONED SHALL BE BULKHEADED AT EACH END AND COMPLETELY FILLED WITH FLOWABLE CONCRETE AS DIRECTED BY THE ENGINEER (PAY ITEM: ABANDON PIPE). IF PIPE HAS A BLIND TIE-IN TO THE EXISTING MAIN, THEN PIPE SHALL BE BULKHEADED AT THE UPSTREAM END ONLY (PAY ITEM: PLUG PIPE).
- ALL SIDEWALKS, DRIVEWAY APRONS AND SIDEWALK RAMP SHALL BE CONSTRUCTED TO PROVIDE HANDICAPPED ACCESSIBILITY IN ACCORDANCE WITH THE CONNECTICUT BUILDING CODE.
- ALL CONCRETE SIDEWALKS SHALL MATCH THE WIDTH AND ELEVATION OF EXISTING, ADJACENT CONCRETE SIDEWALKS. IN GENERAL, ALL CONCRETE SIDEWALKS WITHIN THE RIGHT-OF-WAY ARE TO BE 5" THICK AND 5' WIDE.
- ACTUAL LIMITS OF DRIVEWAY RECONSTRUCTION REQUIRED SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. THE CONTRACTOR SHALL AVOID IMPACTS TO EXISTING DRIVEWAYS AND APRONS WHEREVER POSSIBLE TO MINIMIZE REQUIRED RECONSTRUCTION LIMITS.
- CONCRETE SIDEWALK RAMP ARE TO MATCH EXISTING CURB RADI AND HAVE A 5' MINIMUM RADIUS UNLESS NOTED OTHERWISE. THE ENGINEER WILL PROVIDE STAKES AS REQUIRED DURING CONSTRUCTION FOR LOCATIONS WITH RADII THAT VARY FROM EXISTING.
- ALL CONCRETE SIDEWALK RAMP SHALL BE INSTALLED WITH DETECTABLE WARNING TILES.
- WHERE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL PROVIDE SLEEVES FOR ALL SIGNS LOCATED WITHIN THE LIMITS OF PROPOSED CONCRETE SIDEWALK.
- FOR SIDEWALKS AND RAMP, A CLEARANCE OF 48" (36" MINIMUM) MUST BE PROVIDED BETWEEN ANY OBSTRUCTION AND THE BACK EDGE OF THE SIDEWALK AND RAMP OR FACE OF CURB.
- FINAL LOCATION OF ALL PROPOSED UNDERGROUND UTILITIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL TEMPORARILY STOCKPILE MATERIAL EXCAVATED FROM THE TRENCH ADJACENT TO THE EXCAVATION AND IMMEDIATELY BACKFILL UPON COMPLETION OF THE PIPE INSTALLATION. IF NECESSARY, STOCKPILING OF ADDITIONAL MATERIALS SURROUNDED BY APPROPRIATE EROSION CONTROLS WILL BE COMPLETED WITH APPROVAL OF THE ENGINEER.
- A PAVEMENT CORING PROGRAM WAS COMPLETED WITHIN THE PROJECT AREA IN MARCH 2022. THE CORING LOGS ARE LOCATED IN THE CONTRACT SPECIFICATIONS FOR REFERENCE.
- THE CONTRACTOR SHALL PERFORM TEN (10) TEST PITS IDENTIFIED ON THE PLANS WITHIN 30 CALENDAR DAYS OF RECEIVING THE NOTICE TO PROCEED. CONTACT THE ENGINEERING DIVISION AT (860) 647-5211 AT LEAST 48 HOURS BEFORE PERFORMING TEST PITS. TEST PIT DATA THAT INCLUDES ELEVATIONS, LOCATIONS, DIMENSIONS AND MATERIALS OF ASSOCIATED UTILITIES MUST BE PROVIDED TO THE TOWN IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS IMMEDIATELY AFTER COMPLETION OF FIELD INVESTIGATIONS. HORIZONTAL LOCATION OF EXISTING UTILITIES MUST BE IDENTIFIED BY FIELD SURVEY OR A MINIMUM OF TWO (2) TIES FROM BUILDING OR OTHER FIXED PHYSICAL FEATURE SHOWN ON THE PLANS AND VERTICAL LOCATIONS MUST BE DETERMINED BASED ON PROJECT BENCHMARK ELEVATIONS. ADDITIONAL TEST PITS MAY BE REQUIRED AND HORIZONTAL AND VERTICAL LOCATIONS OF PROPOSED WORK MAY BE ADJUSTED TO FIT EXISTING FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.
- A REDLINED PROGRESS SET OF DRAWINGS SHALL BE MAINTAINED DAILY AND BE AVAILABLE TO THE ENGINEER AT ALL TIMES.
- PAYMENT FOR "REMOVE BITUMINOUS CONCRETE PAVEMENT" SHALL BE FOR AREAS IDENTIFIED ON THE PLANS AND DOES NOT INCLUDE AREAS IDENTIFIED AS FULL-DEPTH RECONSTRUCTION. PAVEMENT REMOVED FROM FULL-DEPTH RECONSTRUCTION AREAS SHALL BE PAID FOR AS "EARTH EXCAVATION".
- THE TOWN IS IN THE PROCESS OF SECURING RIGHTS FROM PRIVATE PROPERTY OWNERS TO ENTER IMPACTED PROPERTIES TO COMPLETE WORK SHOWN ON THE CONTRACT PLANS. THE CONTRACTOR SHALL CONFIRM APPROVALS HAVE BEEN OBTAINED BY THE TOWN PRIOR TO PERFORMING WORK ON PRIVATE PROPERTY.

WATER AND SEWER NOTES

- A CONSTRUCTION SEQUENCE AND PIPE LAYING SCHEDULE SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PRIOR TO ANY CONSTRUCTION ACTIVITY. ALONG ROADS WHERE BOTH UTILITIES WILL BE INSTALLED, SANITARY SEWER FACILITIES SHALL BE INSTALLED PRIOR TO WATER DISTRIBUTION SYSTEM IMPROVEMENTS UNLESS APPROVED OTHERWISE BY THE ENGINEER. DEVIATIONS FROM SAID SCHEDULE OR CONSTRUCTION SEQUENCE SHALL BE APPROVED BY THE ENGINEER. IT IS ANTICIPATED THAT SCHEDULING OF MULTIPLE CREWS FOR SANITARY SEWER AND WATER SYSTEM INSTALLATION WILL BE REQUIRED TO ADHERE TO THE PROJECT SCHEDULE AND MINIMIZE THE DURATION OF IMPACT TO EXISTING UTILITIES SERVING PROPERTIES IN THE AREA.
- UTILITY INSTALLATION WITHIN THE PROJECT AREA WILL IMPACT ACCESS TO RESIDENTIAL AND COMMERCIAL PROPERTIES. THE CONTRACTOR SHALL MAKE EVERY EFFORT TO PROVIDE ACCESS TO ALL PROPERTIES AT ALL TIMES. THIS WORK MAY NEED TO BE COMPLETED ON WEEKENDS AND OFF-HOURS WITH THE APPROVAL OF THE ENGINEER.
- THE CONTRACTOR SHALL SUBMIT A PLAN TO THE ENGINEER FOR PROPOSED METHODS TO DEWATER TRENCHES (WHERE REQUIRED). SUCH PLAN SHALL INCLUDE THE INSTALLATION OF SILT BAGS AT PUMP DISCHARGES WITH SILT FENCE/HAYBALES FOR EROSION CONTROL. PROPOSED DISCHARGE LOCATIONS AND ALL EROSION CONTROLS SHALL BE REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO CONSTRUCTION.
- ALL CONCRETE AND TEMPORARY BITUMINOUS CONCRETE SIDEWALKS SHALL MATCH THE WIDTH AND ELEVATION OF EXISTING, ADJACENT CONCRETE SIDEWALKS.
- FOR CONNECTIONS TO EXISTING SANITARY SEWER STRUCTURES AND PIPING, THE CONTRACTOR SHALL VERIFY EXISTING INVERT ELEVATIONS, NOTIFY THE ENGINEER IF A DISCREPANCY EXISTS, AND ADJUST THE PIPE SLOPES AS DIRECTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR TEMPORARY HANDLING OF ALL SEWAGE

- FLOWING DURING CONSTRUCTION. METHODS OF HANDLING SEWAGE FLOWS SHALL BE APPROVED BY THE ENGINEER.
- AT THE END OF EACH WORKING DAY, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONNECTING EXISTING SANITARY SEWER LATERALS TO NEW SANITARY FACILITIES. THE CONTRACTOR IS RESPONSIBLE FOR PROPER DISPOSAL OF ALL MATERIALS.
- ALL SANITARY SEWER MANHOLES SHALL HAVE A 48" INTERNAL DIAMETER UNLESS SPECIFIED OTHERWISE ON THE PLANS.
- ALL PIPE INSTALLED FOR NEW LATERALS FROM BUILDINGS AND RECONNECTION OF EXISTING LATERALS AT THE STREETLINE SHALL BE 6" PVC. THE LOCATION OF CLEANOUTS, PIPE FITTINGS AND SEWER CHIMNEYS AND THE SLOPE OF ALL LATERALS SHALL BE AS DIRECTED BY THE ENGINEER DURING CONSTRUCTION. INSTALL LATERALS TO PROVIDE 18" MINIMUM CLEARANCE BELOW EXISTING WATER MAINS UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- LOCATION, SIZE AND PIPE MATERIALS SHOWN ON THE PLANS FOR EXISTING SANITARY CONNECTIONS TO THE EXISTING SANITARY SEWER SYSTEM, AT PROPER TYPICAL LINES, AND NEAR EXISTING HOUSES ARE APPROXIMATE BASED ON VIDEO INSPECTIONS AND AVAILABLE MAPPING. IF ADDITIONAL INFORMATION IS REQUIRED, AT THE REQUEST OF THE CONTRACTOR, THE TOWN WILL ASSIST IN LOCATING LATERALS BY THE USE OF RODS AND AN ELECTRONIC LOCATOR.
- LOCATION OF PROPOSED CONNECTIONS TO EXISTING SANITARY LATERALS ARE ONLY APPROXIMATE BASED ON AVAILABLE MAPPING AND BUILDING INSPECTIONS. CONTRACTOR SHALL PERFORM TEST PITS AS NOTED ON THE PLANS.
- AT ALL WATER AND SEWER UTILITY CROSSINGS, A MINIMUM 18" VERTICAL AND 10' HORIZONTAL SEPARATION DISTANCE SHALL BE PROVIDED UNLESS INDICATED OTHERWISE ON THE PLANS OR APPROVED BY THE ENGINEER. IF THIS SEPARATION DISTANCE CANNOT BE ACHIEVED, THE ENGINEER SHALL BE NOTIFIED PRIOR TO PROCEEDING.
- AS DIRECTED BY THE ENGINEER, A CONCRETE PIPE CRADLE SHALL BE INSTALLED WHEN THE VERTICAL CLEARANCE BETWEEN WATER, SANITARY SEWER, DRAINAGE AND OTHER UTILITY PIPING IS LESS THAN OR EQUAL TO 12" AND CRUSHED STONE SHALL BE USED WHENEVER THE VERTICAL CLEARANCE IS GREATER THAN 12" BUT LESS THAN OR EQUAL TO 18", IN ACCORDANCE WITH THE ASSOCIATED UTILITY SUPPORT DETAIL.
- RELOCATION AND RECONNECTION OF EXISTING WATER SERVICES MAY BE REQUIRED DUE TO CONFLICTS WITH PROPOSED SANITARY SEWER. ALL RELOCATED SERVICES SHALL HAVE 4-1/2 FEET OF COVER AND MUST BE INSTALLED AS DIRECTED BY THE ENGINEER.
- ALL EXISTING HYDRANTS AND FRAMES AND COVERS FROM SANITARY SEWER MANHOLES TO BE REMOVED, REGRADED OR ABANDONED SHALL BE SALVAGED. ALL OTHER MATERIALS WHICH ARE REMOVED FROM THE SITE BECOME PROPERTY OF THE CONTRACTOR, AND ARE TO BE DISPOSED OF PROPERLY, UNLESS INDICATED OTHERWISE ON THE PLANS. ALL MATERIALS THE ENGINEER DESIGNATES TO BE SALVAGED SHALL BE DELIVERED TO THE WATER & SEWER DEPARTMENT GARAGE ON CHARTER OAK STREET, MANCHESTER, CT AND PROPERLY UNLOADED BY THE CONTRACTOR.
- ALL VALVE BOXES AND CURB BOXES SHALL BE ADJUSTED TO MATCH FINISHED GRADES. ALL CURB BOXES SHALL BE LOCATED IN GRASSED AREAS AT THE STREET LINE FRONTING THE PROPERTY UNLESS INDICATED OTHERWISE ON THE PLANS.
- ALL NEW WATER MAINS AND SERVICES SHALL HAVE 4.5 FEET OF COVER UNLESS OTHERWISE INDICATED ON THE PLANS. COVER LESS THAN OR IN EXCESS OF 4.5 FEET SHALL BE ALLOWED ONLY WHERE INDICATED ON THE PLANS OR APPROVED BY THE ENGINEER. WATER MAINS AND SERVICES WITH COVER LESS THAN 4.5 FEET SHALL BE INSULATED UNLESS APPROVED OTHERWISE BY THE ENGINEER.
- THRUST RESTRAINT FOR ALL MECHANICAL JOINTS AT VALVES AND FITTINGS SHALL BE PROVIDED BY MEANS OF DUCTILE IRON RESTRAINER GLANDS. WEDGE-JOINT JOINT RESTRAINERS OR GASKET-TYPE JOINT RESTRAINT SHALL BE USED TO RESTRAIN ALL DUCTILE IRON PIPE JOINTS FOR A DISTANCE OF AT LEAST 27 FEET ON EACH SIDE OF ALL VALVES OR FITTINGS. NO MORE THAN ONE PIPE JOINT SHALL BE ALLOWED WITHIN THAT 27 FEET OF PIPE.
- ALL EXISTING WATER SERVICES SHALL BE RECONNECTED TO THE NEW FACILITIES. ALL WATER SERVICES SHALL BE NEW 1" COPPER TUBING FROM THE MAIN TO THE CURB BOX UNLESS OTHERWISE INDICATED ON THE PLANS. THE CURB BOX SHALL BE REPLACED AS PART OF THE INSTALLATION.
- EXISTING WATER MAINS SHOWN ARE NOT SHOWN IN PROFILES FOR CLARITY. ALL EXISTING WATER MAINS ARE ASSUMED TO HAVE 4.5-5 FEET OF COVER. LOCATIONS AND APPROXIMATE DEPTHS FOR OTHER UTILITIES WERE PROVIDED BY THE INDIVIDUAL UTILITY COMPANIES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY TEMPORARY THRUST RESTRAINT THAT IS REQUIRED.
- ALL VALVES AND HYDRANTS SHALL BE RIGHT-OPENING (CLOCKWISE) UNLESS INDICATED OTHERWISE. ALL MAIN LINE AND AUXILIARY VALVES (4" TO AND INCLUDING 12") SHALL BE RESILIENT WEDGE GATE VALVES.
- EXISTING WATER AND SEWER MAINS INDICATED ON THE PLANS TO BE ABANDONED SHALL BE REMOVED WHEN SUCH MAINS ARE LOCATED WITHIN THE HORIZONTAL TRENCH LIMITS OF NEW PIPE TO BE INSTALLED IN THE SAME LOCATION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE LINEAR FOOT COST FOR THE ASSOCIATED PIPE INSTALLATION.
- IT IS ANTICIPATED THAT A TEMPORARY ABOVE GROUND BYPASS SYSTEM WILL BE REQUIRED DUE TO AFFECTED PROPERTIES FOR THE DURATION OF THE PROJECT. A PLAN FOR THE TEMPORARY BYPASS SYSTEM SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL AT LEAST TWO (2) WEEKS PRIOR TO ITS INTENDED USE. NO CONSTRUCTION SHALL BEGIN UNTIL THE PLAN, WHICH MUST INCLUDE ALL PROVISIONS AND REQUIREMENTS IDENTIFIED IN THE CONTRACT SPECIFICATIONS, HAS BEEN REVIEWED AND APPROVED BY THE ENGINEER.
- WHEN DIRECTED BY THE ENGINEER, TEMPORARY FIRE HYDRANTS SHALL BE PROVIDED WHERE EXISTING HYDRANTS ARE OUT OF SERVICE DUE TO CONSTRUCTION. THE LOCATION OF TEMPORARY FIRE HYDRANTS SHALL BE REVIEWED AND APPROVED BY THE ENGINEER. THERE WILL BE NO SEPARATE PAYMENT FOR TEMPORARY HYDRANTS, THEIR COST SHALL BE INCLUDED IN THE BID PRICE FOR "WATER BYPASS PIPING" OF THE SIZE SPECIFIED.
- EVEN WHEN NOT SPECIFICALLY LABELED ON THE CONTRACT PLANS, EXISTING SIDEWALKS, DRIVEWAYS, DRIVEWAY APRONS, CURBING AND LAWN AREAS DISTURBED DURING INSTALLATION OF SANITARY SEWER AND WATER FACILITIES SHALL BE REPAIRED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS AS DIRECTED BY THE ENGINEER DURING CONSTRUCTION AND PAID FOR UNDER THE ASSOCIATED CONTRACT ITEMS.



TYPICAL ROADWAY SECTION

NOT TO SCALE

NOTES:

- THE CROSS SLOPE AND DIRECTION OF THE ROADWAY SHOWN IN THE TYPICAL SECTION DOES NOT REPRESENT PROPOSED CONSTRUCTION. IN GENERAL, THE CONTRACTOR SHALL MATCH THE EXISTING CROSS SLOPE AND DIRECTION OF THE ROAD SURFACE IN THE FIELD.
- ALL GRASS SHELVES AND SLOPES BEHIND SIDEWALKS SHALL BE GRADED TO DRAIN TO THE ROAD AT A MINIMUM SLOPE OF 2% UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
- FOR ADDITIONAL INFORMATION, REFER TO THE ROAD PAVEMENT CORE DATA PROVIDED IN THE CONTRACT SPECIFICATIONS THAT INDICATES EXISTING PAVEMENT THICKNESSES.



**TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION**

494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- = METLAND BOUNDARY	☆ = LIGHT POLE
--- = RETAINING WALL	⊗ = CONIFEROUS TREE
--- = GUIDE RAIL	⊗ = DECIDUOUS TREE
⊗ = STONE WALL	⊗ = SANITARY MANHOLE
⊗ = STOCKADE FENCE	⊗ = DRAINAGE MANHOLE
--- = WIRE FENCE	⊗ = CATCH BASIN
--- = CHAIN LINK FENCE	⊗ = CULVERT END
--- = PROPERTY LINE	⊗ = HYDRANT
--- = RAILROAD TRACKS	⊗ = CURB STOP
--- = SILT FENCE	⊗ = WATER VALVE
⊗ = CONCRETE MONUMENT	⊗ = BUTTERFLY VALVE
⊗ = GRANITE MONUMENT	⊗ = BLOW OFF
⊗ = IRON PIPE	⊗ = SIGN
⊗ = IRON ROD	⊗ = DOUBLE POST SIGN
⊗ = CONTROL POINT	⊗ = MAIL BOX
⊗ = DRILL HOLE	⊗ = BOLLARD
⊗ = UTILITY POLE	⊗ = CONTROLLER CABINET
⊗ = UTILITY POLE WITH LIGHT	⊗ = TRAFFIC SIGNAL POLE
⊗ = ELECTRIC BOX	⊗ = GAS GATE
⊗ = METLAND FLAG	⊗ = TELEPHONE BOX
	⊗ = CATV TUBE

PROJECT NUMBER
2020088

FILENAME
2020088PLAN.DWG

NO.	DATE	FILE
1	06/10/22	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JED
RELEASED BY: JED

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HORIZONTAL: --- = 20'
OR AS NOTED
VERTICAL: --- = 20'

GRAPHIC SCALE

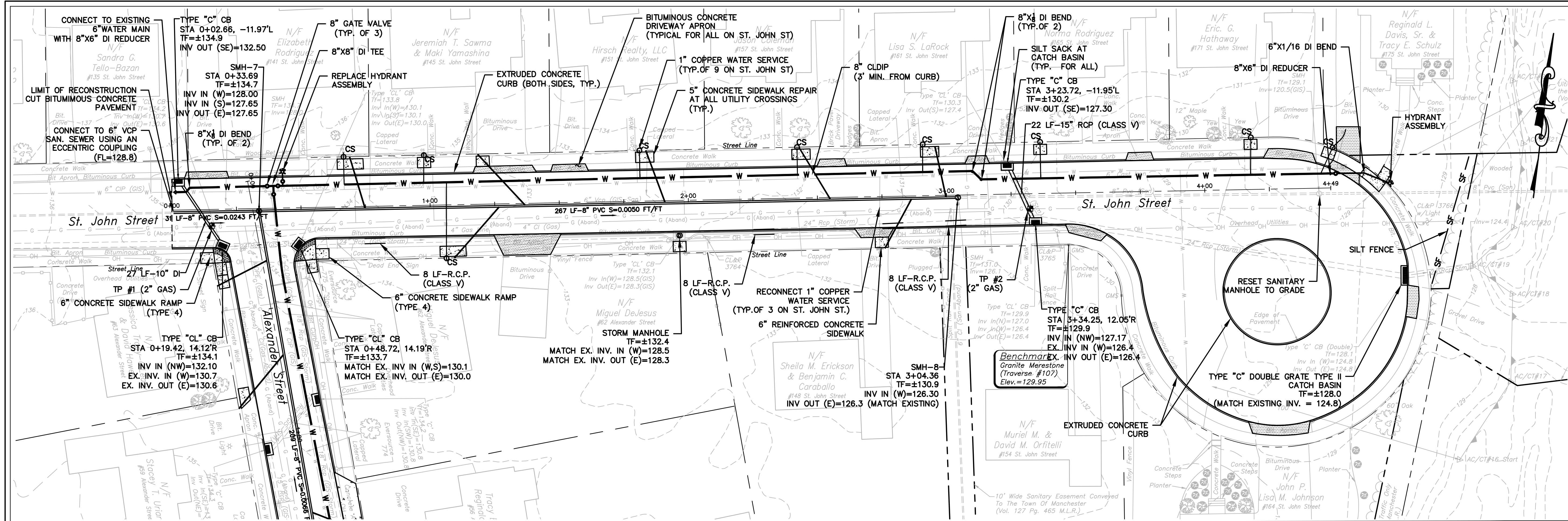
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HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGEWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGEWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
**GENERAL NOTES &
TYPICAL SECTION**

SHEET NUMBER
2 of 20



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- METLANS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊗ CONIFEROUS TREE
--- GUIDE RAIL	⊗ DECIDUOUS TREE
--- STONE WALL	⊗ SANITARY MANHOLE
--- STOCKADE FENCE	⊗ DRAINAGE MANHOLE
--- WIRE FENCE	⊗ CATCH BASIN
--- CHAIN LINK FENCE	⊗ CURB END
--- PROPERTY LINE	⊗ HYDRANT
--- RAILROAD TRACKS	⊗ CURB STOP
--- SILT FENCE	⊗ WATER VALVE
□ CONCRETE MONUMENT	⊗ BUTTERFLY VALVE
● GRANITE MONUMENT	⊗ BLOW OFF
● IRON PIPE	⊗ SIGN
● IRON ROD	⊗ DOUBLE POST SIGN
△ CONTROL POINT	⊗ MAIL BOX
○ DRILL HOLE	⊗ BOLLARD
○ UTILITY POLE	⊗ CONTROLLER CABINET
○ UTILITY POLE WITH LIGHT	⊗ TRAFFIC SIGNAL POLE
⊗ TRAFFIC SIGNAL POLE	⊗ GAS GATE
⊗ ELECTRIC BOX	⊗ TELEPHONE BOX
△ WETLAND FLAG	⊗ CATV TUBE

PROJECT NUMBER
2020088

FILENAME
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NO.	DATE	FILE
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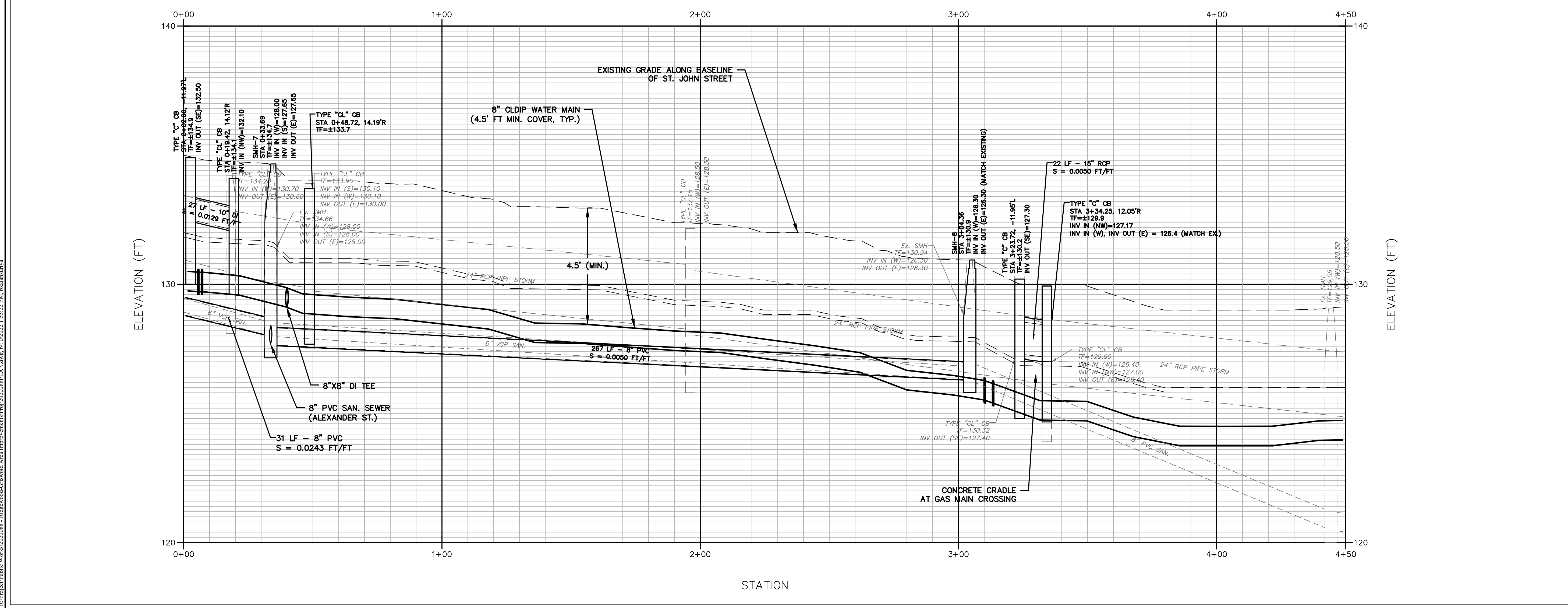
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HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
**ST. JOHN STREET
PLAN & PROFILE**

SHEET NUMBER
3 of 20



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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- METLANDS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊗ CONIFEROUS TREE
--- CLDIP RAIL	⊗ DECEADOUS TREE
--- STONE WALL	⊗ SANITARY MANHOLE
--- STOCKADE FENCE	⊗ DRAINAGE MANHOLE
--- WIRE FENCE	⊗ CATCH BASIN
--- CHAIN LINK FENCE	⊗ HYDRANT
--- PROPERTY LINE	⊗ CURB STOP
--- RAILROAD TRACKS	⊗ WATER VALVE
--- SILT FENCE	⊗ BUTTERFLY VALVE
⊗ CONCRETE MONUMENT	⊗ BLOW OFF
⊗ GRANITE MONUMENT	⊗ SIGN
⊗ IRON PIPE	⊗ DOUBLE POST SIGN
⊗ IRON ROD	⊗ MAIL BOX
⊗ CONTROL POINT	⊗ BOLLARD
⊗ DRILL HOLE	⊗ UTILITY POLE WITH LIGHT
⊗ UTILITY POLE	⊗ TRAFFIC SPAN POLE
⊗ TRAFFIC SPAN POLE	⊗ GAS GATE
⊗ ELECTRIC BOX	⊗ TELEPHONE BOX
⊗ WETLAND FLAG	⊗ CATV TUBE

PROJECT NUMBER
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FILENAME
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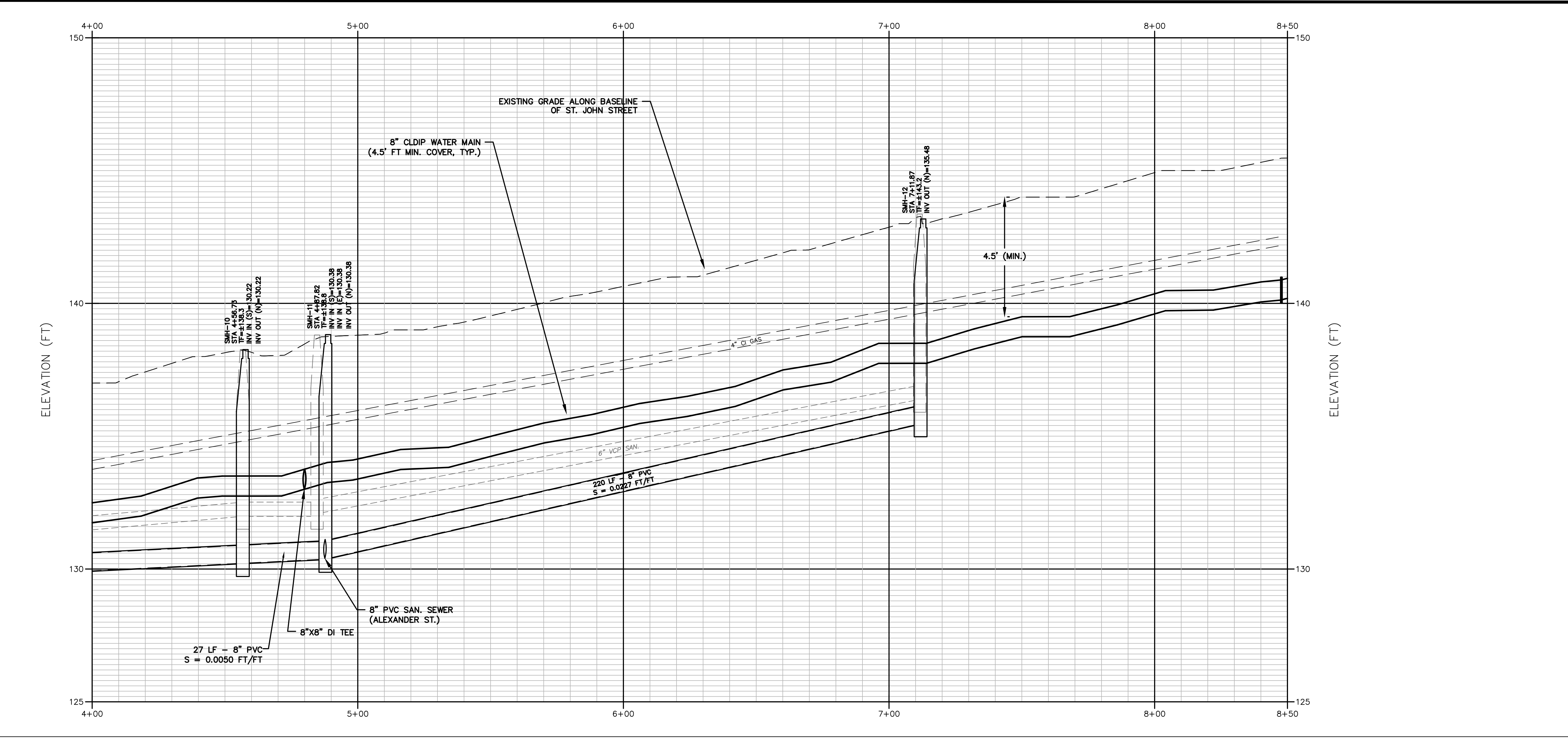
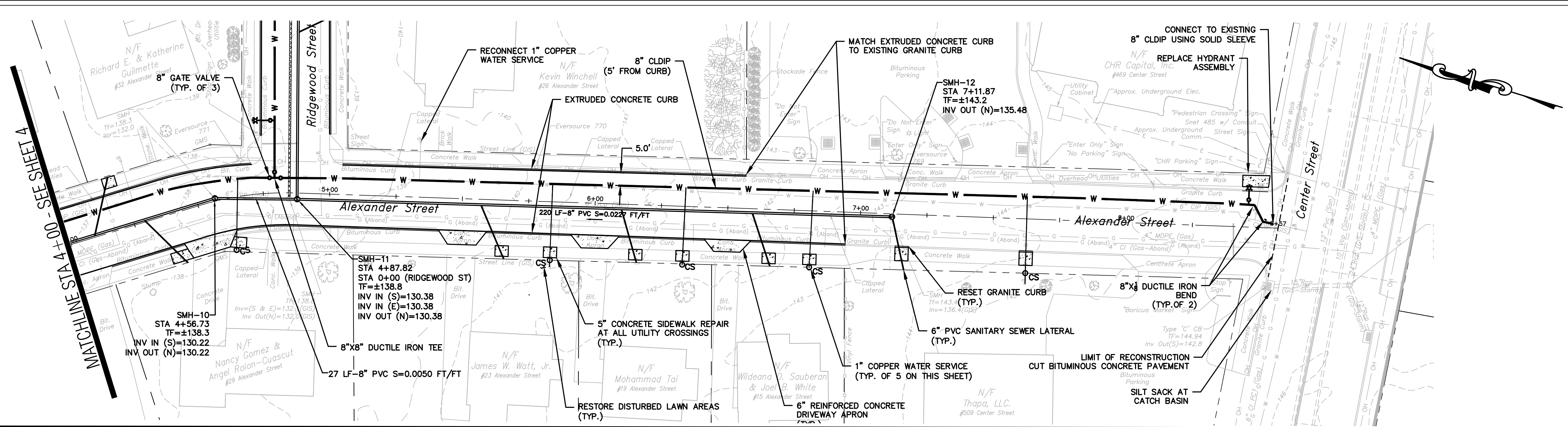
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HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
**ALEXANDER STREET
PLAN & PROFILE
(STA. 4+00 TO STA 8+50)**

SHEET NUMBER
5 of 20



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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- METLANDS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊗ CONIFEROUS TREE
--- CURB WALL	⊗ DECIDUOUS TREE
--- STONE WALL	⊗ SANITARY MANHOLE
--- STOCKADE FENCE	⊗ DRAINAGE MANHOLE
--- WIRE FENCE	⊗ CATCH BASIN
--- CHAIN LINK FENCE	⊗ CULVERT END
--- PROPERTY LINE	⊗ HYDRANT
--- RAILROAD TRACKS	⊗ CURB STOP
--- SILT FENCE	⊗ WATER VALVE
⊗ CONCRETE MONUMENT	⊗ BUTTERFLY VALVE
⊗ GRANITE MONUMENT	⊗ BLOW OFF
⊗ IRON PIPE	⊗ SIGN
⊗ IRON ROD	⊗ DOUBLE POST SIGN
⊗ CONTROL POINT	⊗ MAIL BOX
⊗ DRILL HOLE	⊗ BOLLARD
⊗ UTILITY POLE	⊗ CONTROLLER CABINET
⊗ UTILITY POLE WITH LIGHT	⊗ TRAFFIC SIGNAL POLE
⊗ TRAFFIC SIGNAL POLE	⊗ GAS GATE
⊗ ELECTRIC BOX	⊗ TELEPHONE BOX
⊗ WETLAND FLAG	⊗ CATV TUBE

PROJECT NUMBER
2020088

FILENAME
2020088PLAN.DWG

NO.	DATE	FILE
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CHECKED BY: JED
RELEASED BY: JED

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OR AS NOTED
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GRAPHIC SCALE

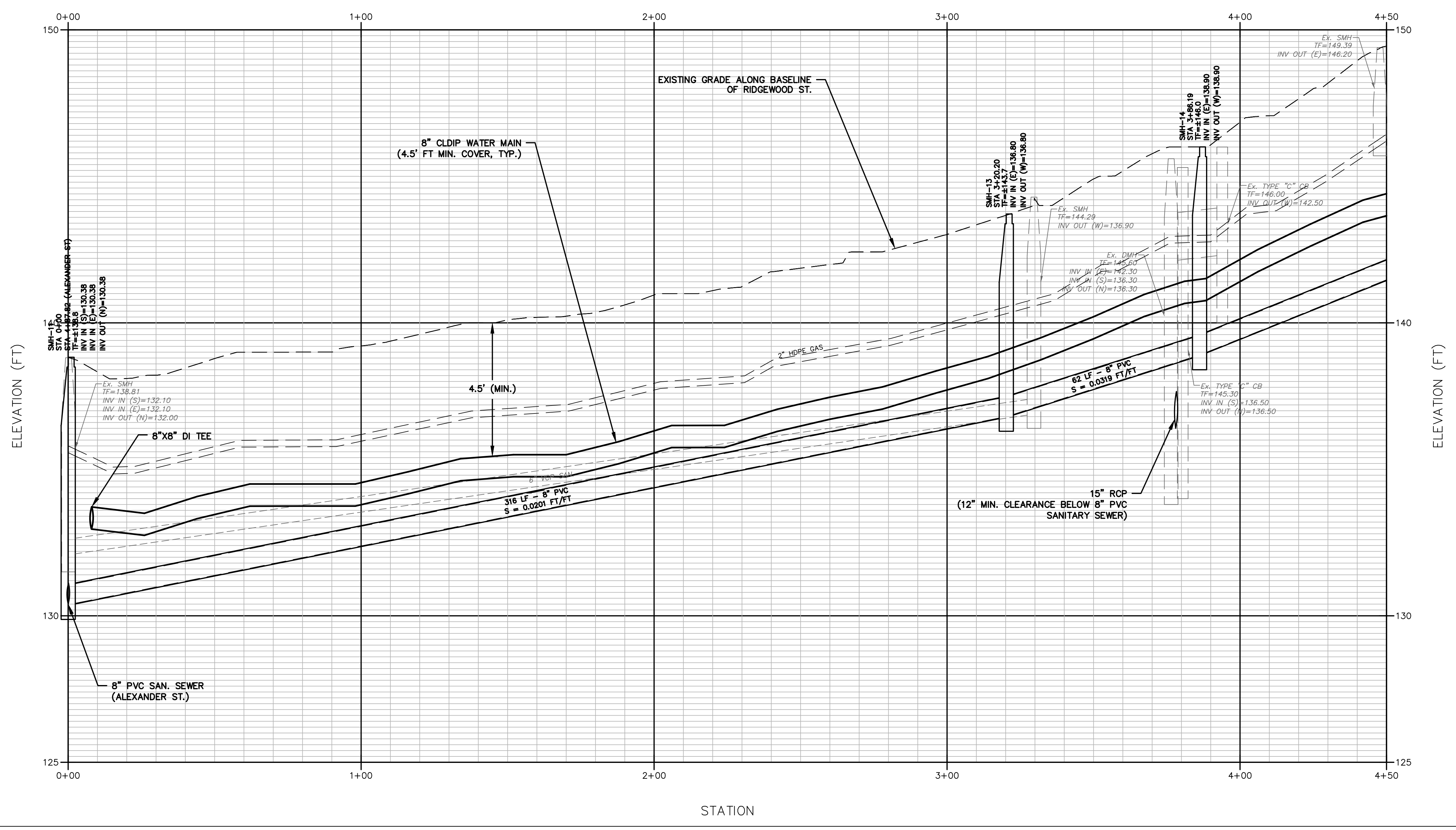
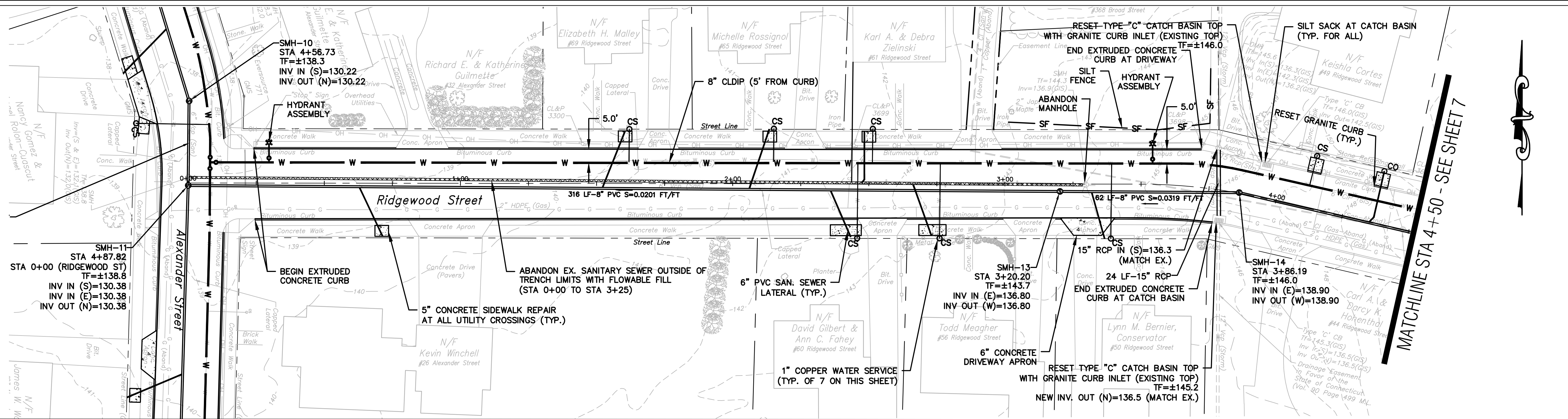
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PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

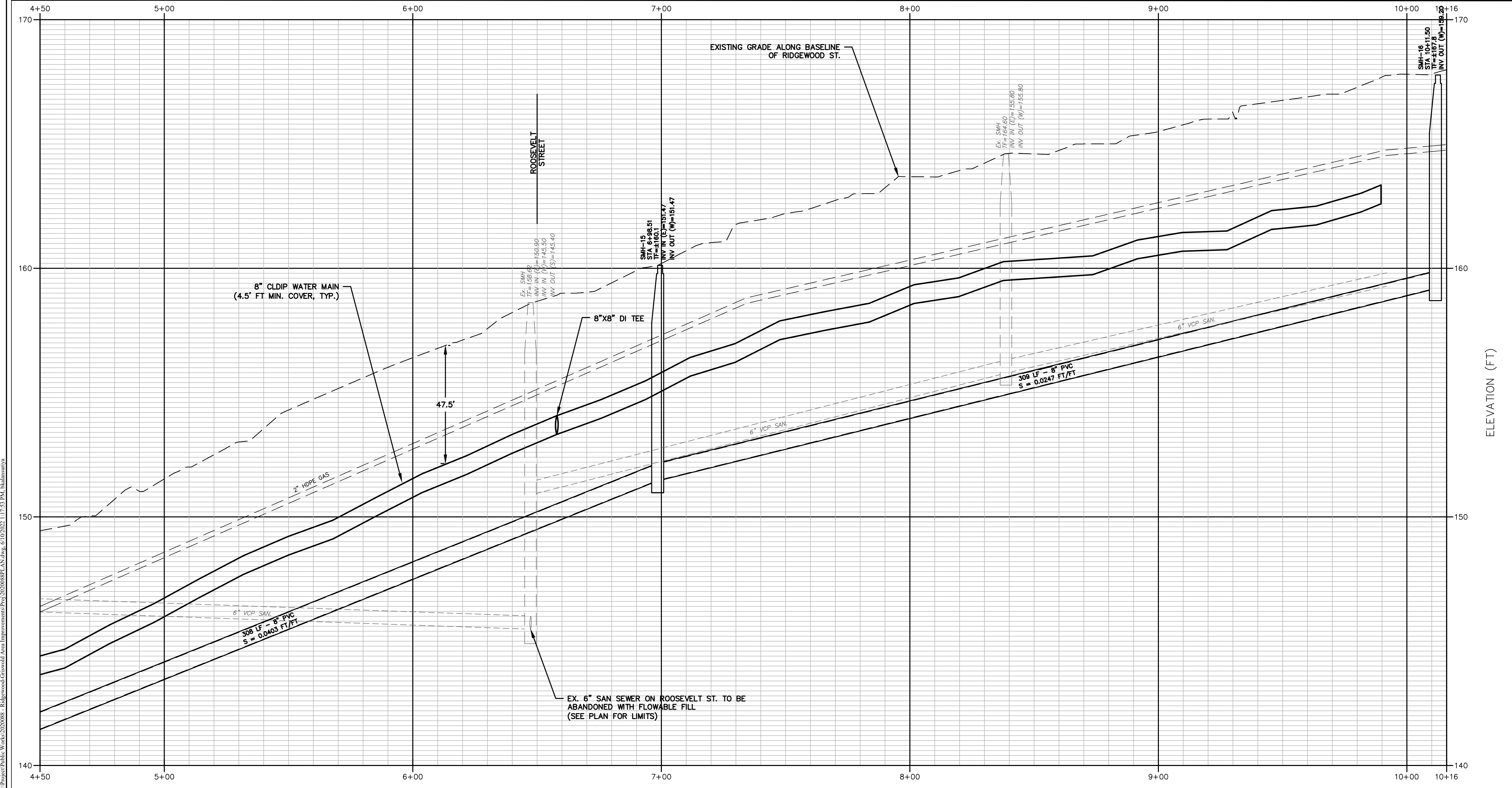
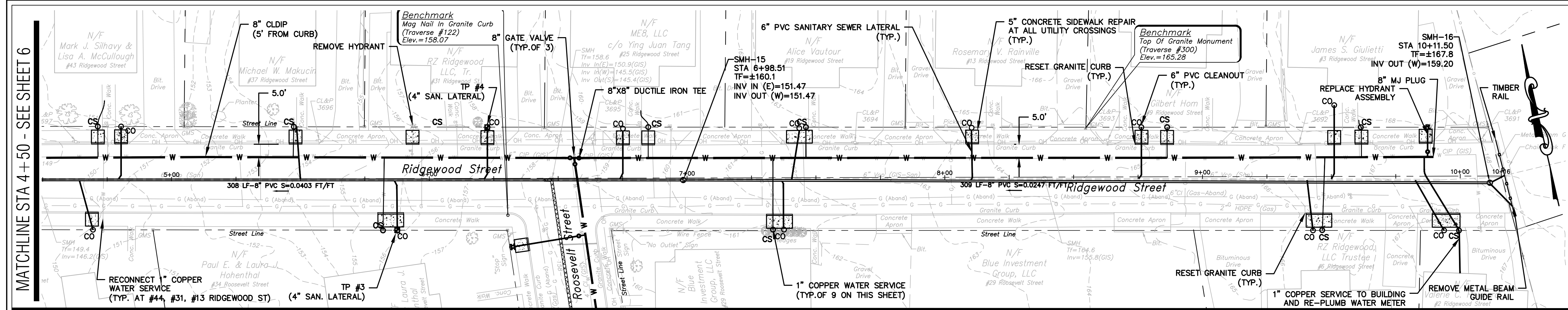
SHEET TITLE
**RIDGWOOD ST.
PLAN & PROFILE
(STA 0+00 TO STA. 4+50)**

SHEET NUMBER
6 of 20



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MATCHLINE STA 4+50 - SEE SHEET 6



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

--- WETLANDS BOUNDARY	☆ LIGHT POLE
--- RETAINING WALL	⊗ CONIFEROUS TREE
--- GRADE RAIL	⊗ DECIDUOUS TREE
--- STONE WALL	⊗ SANITARY MANHOLE
--- STOCKADE FENCE	⊗ DRAINAGE MANHOLE
--- WIRE FENCE	⊗ CATCH BASIN
--- CHAIN LINK FENCE	⊗ CURB END
--- PROPERTY LINE	⊗ HYDRANT
--- RAILROAD TRACKS	⊗ CURB STOP
--- SILT FENCE	⊗ WATER VALVE
⊠ CONCRETE MONUMENT	⊗ BUTTERFLY VALVE
⊠ GRANITE MONUMENT	⊗ BLOW OFF
● IRON PIPE	⊗ SIGN
● IRON ROD	⊗ DOUBLE POST SIGN
△ CONTROL POINT	⊗ MAIL BOX
○ DRILL HOLE	⊗ BOLLARD
⊗ UTILITY POLE	⊗ CONTROLLER CABINET
⊗ TRAFFIC SPAN POLE	⊗ GAS GATE
⊗ ELECTRIC FLAG	⊗ TELEPHONE BOX
⊗ WETLAND FLAG	⊗ CATV TUBE

PROJECT NUMBER
2020088

FILENAME
2020088PLAN.DWG

NO.	DATE	FILE
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CHECKED BY: JED
RELEASED BY: JED

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OR AS NOTED
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GRAPHIC SCALE

DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGEWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGEWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

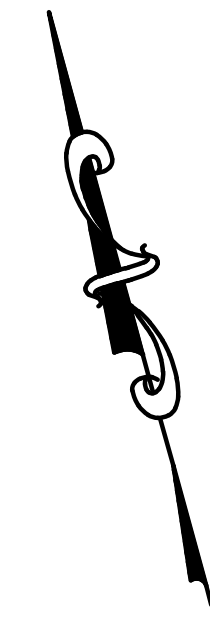
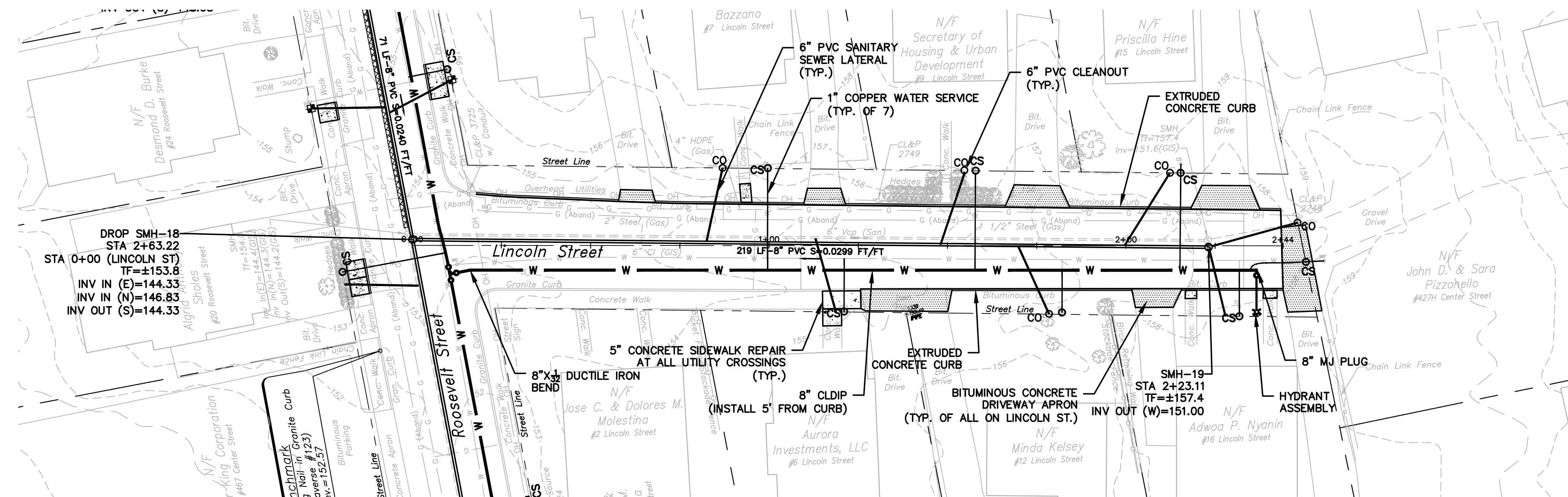
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**RIDGEWOOD ST.
PLAN & PROFILE
(STA 4+50 TO STA. 10+16)**

SHEET NUMBER
7 of 20

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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191



LEGEND

---	WETLANDS BOUNDARY	☆	LIGHT POLE
---	RETAINING WALL	⊗	CONIFEROUS TREE
---	GLIDE RAIL	⊗	DECIDUOUS TREE
---	STONE WALL	⊗	SANITARY MANHOLE
---	STOCKADE FENCE	⊗	DRAINAGE MANHOLE
---	WIRE FENCE	⊗	CATCH BASIN
---	CHAIN LINK FENCE	⊗	VALVE END
---	PROPERTY LINE	⊗	HYDRANT
---	RAILROAD TRACKS	⊗	CURB STOP
---	SILT FENCE	⊗	WATER VALVE
---	CONCRETE MONUMENT	⊗	BUTTERFLY VALVE
---	GRANITE MONUMENT	⊗	BLOW OFF
---	IRON PIPE	⊗	SIGN
---	IRON ROD	⊗	DOUBLE POST SIGN
---	CONTROL POINT	⊗	MAIL BOX
---	DRILL HOLE	⊗	BOLLARD
---	UTILITY POLE	⊗	CONTROLLER CABINET
---	UTILITY POLE WITH LIGHT	⊗	GAS GATE
---	TRAFFIC SPAN POLE	⊗	ELECTRIC BOX
---	ELECTRIC BOX	⊗	TELEPHONE BOX
---	WETLAND FLAG	⊗	CATV TUBE

PROJECT NUMBER
2020088

FILENAME
2020088PLAN.DWG

NO.	DATE	FILE
1	06/10/22	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JED
RELEASED BY: JED

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HORIZONTAL: 1" = 20'
VERTICAL: ---
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GRAPHIC SCALE

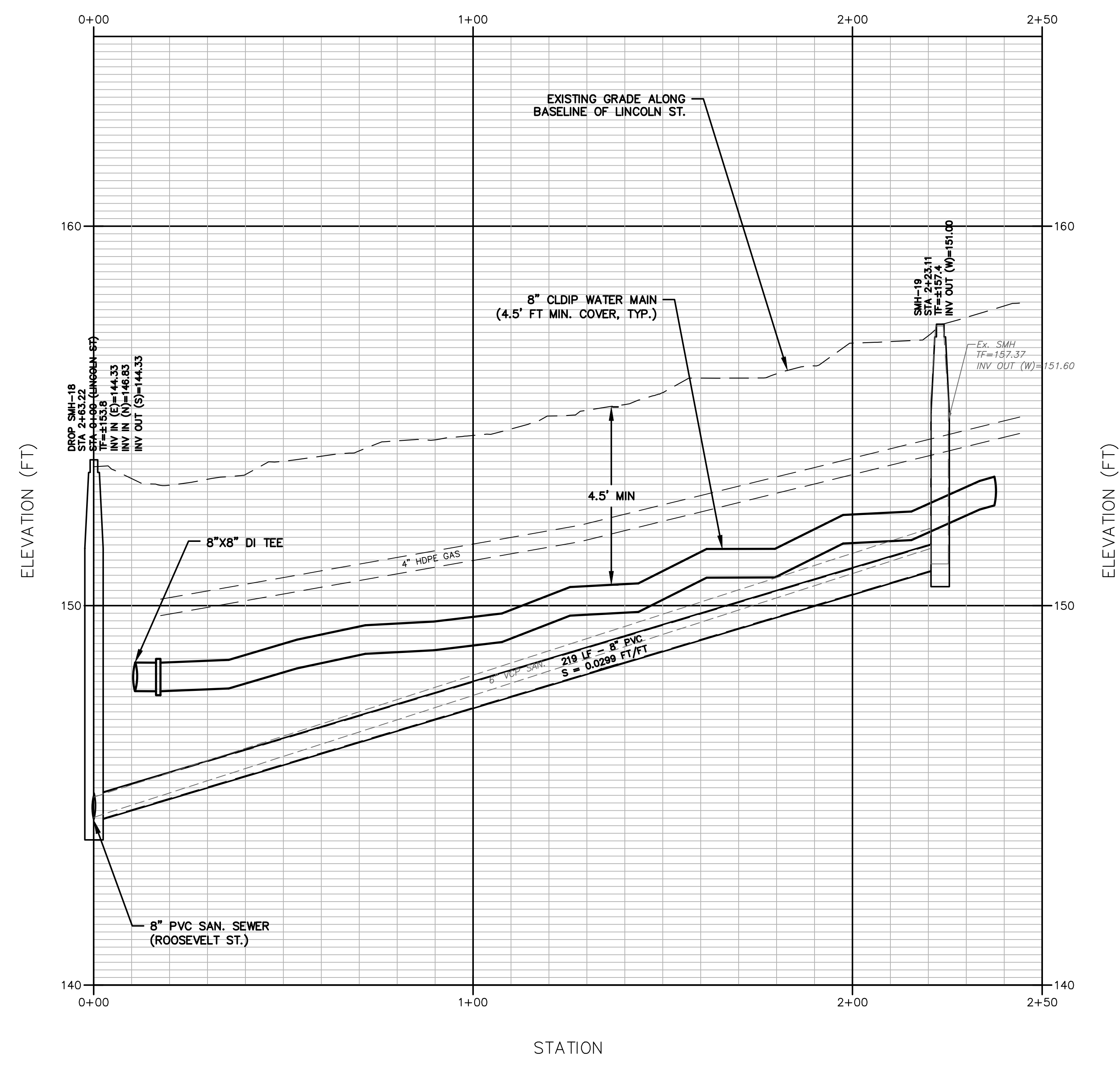
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PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

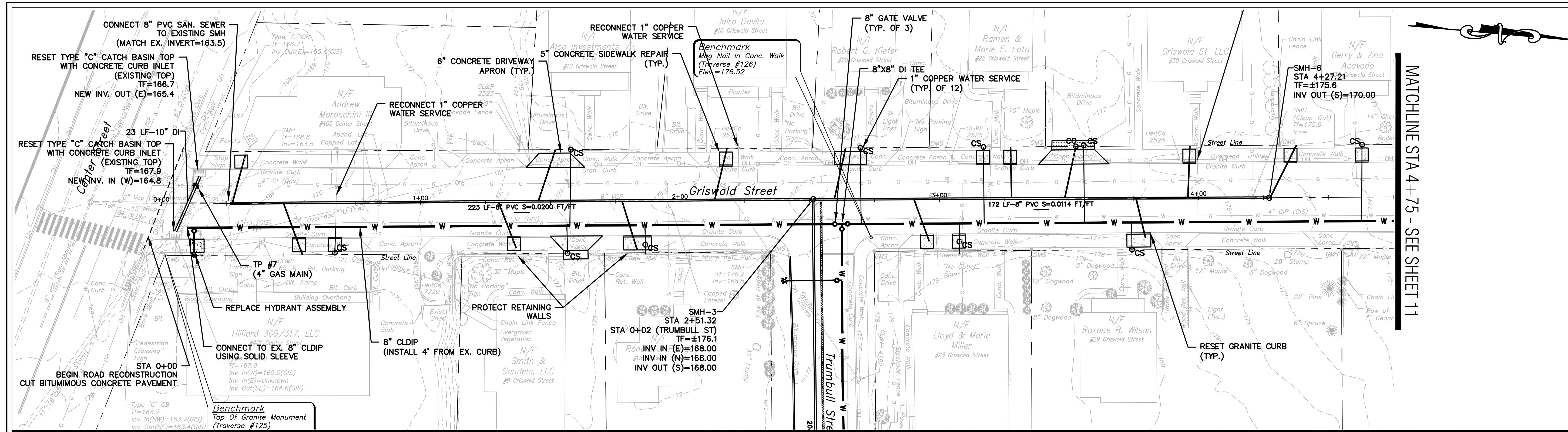
PROJECT TITLE
**RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
**LINCOLN STREET
PLAN & PROFILE**

SHEET NUMBER
9 of 20



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MATCHLINE STA 4 + 75 - SEE SHEET 11



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	WETLANDS BOUNDARY	☆	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	GLIDE RAIL	⊙	DECIDUOUS TREE
—	STONE WALL	⊕	SANITARY MANHOLE
—	STOCKADE FENCE	⊗	DRAINAGE MANHOLE
—	WIRE FENCE	⊕	CATCH BASIN
—	CHAIN LINK FENCE	⊕	VALVE END
—	PROPERTY LINE	⊕	HYDRANT
—	RAILROAD TRACKS	⊕	CURB STOP
—	SILT FENCE	⊕	WATER VALVE
⊕	CONCRETE MONUMENT	⊕	BUTTERFLY VALVE
⊕	GRANITE MONUMENT	⊕	BLOW OFF
⊕	IRON PIPE	⊕	SILO
⊕	IRON ROD	⊕	DOUBLE POST SIGN
⊕	CONTROL POINT	⊕	MAIL BOX
⊕	DRILL HOLE	⊕	BOLLARD
⊕	UTILITY POLE	⊕	CONTROLLER CABINET
⊕	UTILITY POLE WITH LIGHT	⊕	TRAFFIC SPAN POLE
⊕	TRAFFIC SPAN POLE	⊕	ELECTRIC BOX
⊕	ELECTRIC BOX	⊕	TELEPHONE BOX
⊕	WETLAND FLAG	⊕	CATV TUBE

PROJECT NUMBER
2020088

FILENAME
2020088PLAN.DWG

NO.	DATE	FILE
—	06/10/22	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JED
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OR AS NOTED
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GRAPHIC SCALE

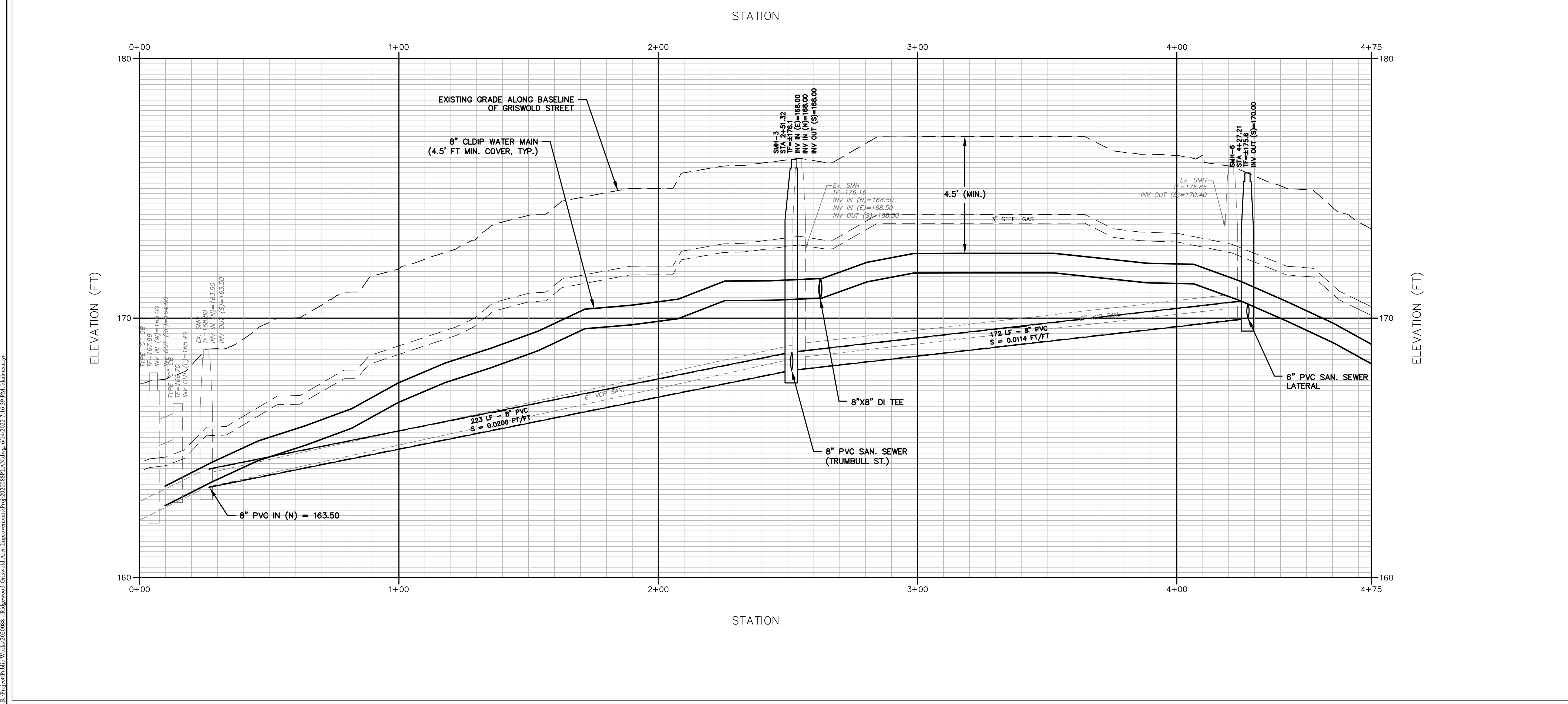
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PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGEWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGEWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
**GRISWOLD STREET
PLAN & PROFILE
STA 0+00 TO STA 4+75**

SHEET NUMBER
10 of 20



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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

---	WETLANDS BOUNDARY	☆	LIGHT POLE
---	RETAINING WALL	⊗	CONIFEROUS TREE
---	GLIDE RAIL	⊙	DECIDUOUS TREE
---	STONE WALL	⊕	SEWERY MANHOLE
---	STOCKADE FENCE	⊖	DRAINAGE MANHOLE
---	WIRE FENCE	⊗	CATCH BASIN
---	CHAIN LINK FENCE	⊕	VALVE END
---	PROPERTY LINE	⊖	HYDRANT
---	RAILROAD TRACKS	⊗	CURB STOP
---	SILT FENCE	⊕	WATER VALVE
---	CONCRETE MONUMENT	⊖	BUTTERFLY VALVE
---	GRANITE MONUMENT	⊕	BLOW OFF
---	IRON PIPE	⊖	SIEN
---	IRON ROD	⊕	DOUBLE POST SIGN
---	CONTROL POINT	⊖	MAIL BOX
---	DRILL HOLE	⊕	BOLLARD
---	UTILITY POLE	⊖	CONTROLLER CABINET
---	UTILITY POLE WITH LIGHT	⊕	TRAFFIC SPAN POLE
---	TRAFFIC SPAN POLE	⊖	GAS GATE
---	ELECTRIC BOX	⊕	TELEPHONE BOX
---	WETLAND FLAG	⊖	CATV TUBE

PROJECT NUMBER
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FILENAME
2020088PLAN.DWG

NO.	DATE	FILE
-	06/10/22	FOR BIDDING

DRAWN BY: BK
CHECKED BY: JED
RELEASED BY: JED

DRAWING SCALE
HORIZONTAL: 1" = 20' VERTICAL: ---
OR AS NOTED
GRAPHIC SCALE

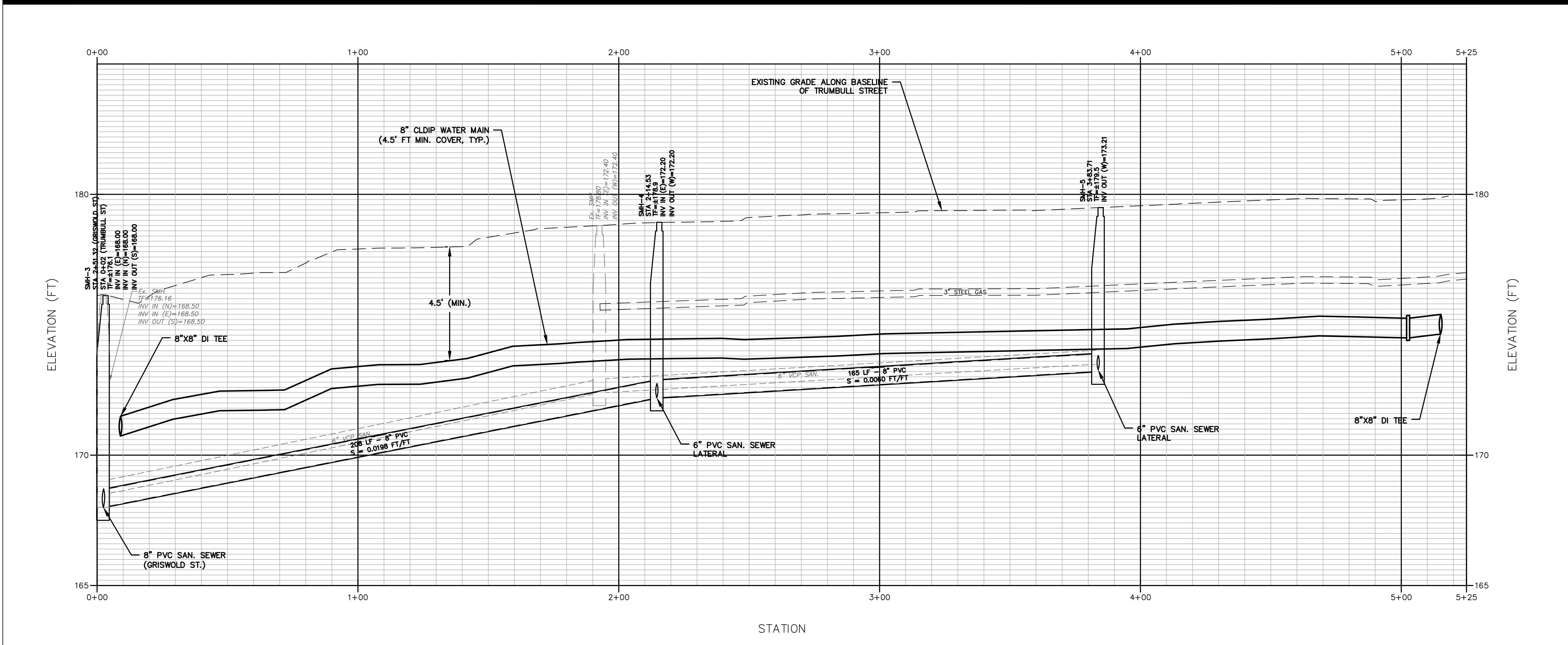
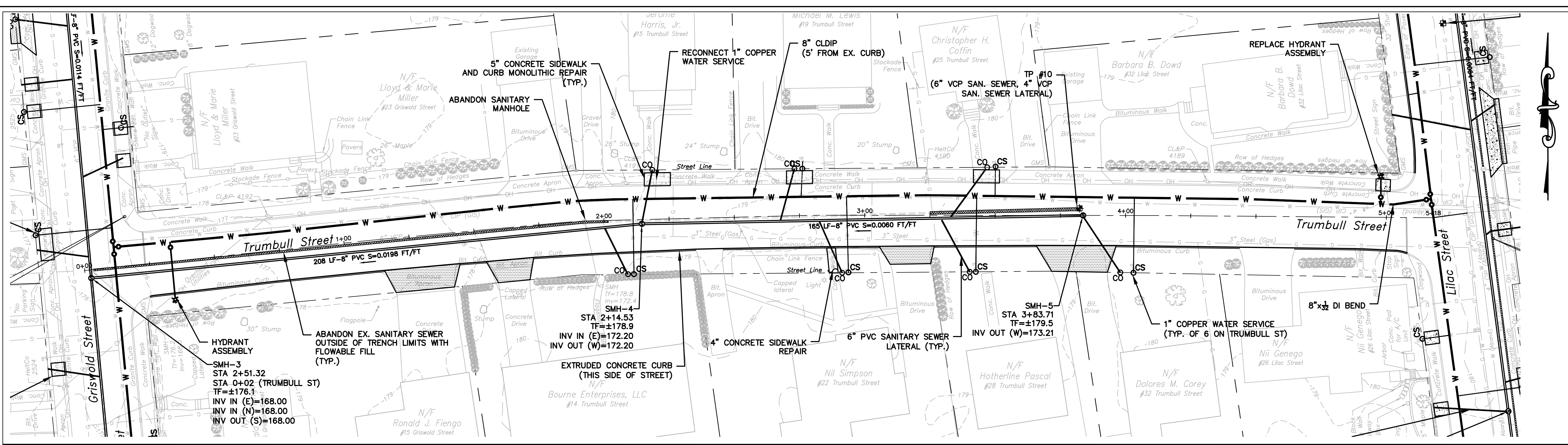
DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
RIDGWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
**TRUMBULL STREET
PLAN & PROFILE**

SHEET NUMBER
12 of 20



R:\Project\Public Works\2020088 - Ridgwood-Griswold Area Improvements\Proj\2020088PLAN.DWG 6/14/2022 10:24 AM bhalmurria



TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

---	WETLANDS BOUNDARY	☆	LIGHT POLE
---	RETAINING WALL	⊗	CONIFEROUS TREE
---	GRADE RAIL	⊙	DECIDUOUS TREE
---	STONE WALL	⊕	SANITARY MANHOLE
---	STOCKADE FENCE	⊗	DRAINAGE MANHOLE
---	WIRE FENCE	⊕	CATCH BASIN
---	CHAIN LINK FENCE	⊗	CHURNY END
---	PROPERTY LINE	⊕	HYDRANT
---	RAILROAD TRACKS	⊗	CURB STOP
---	SILT FENCE	⊕	WATER VALVE
---	CONCRETE MONUMENT	⊗	BUTTERFLY VALVE
---	GRANITE MONUMENT	⊕	BLOW OFF
---	IRON PIPE	⊗	SIEN
---	IRON ROD	⊕	DOUBLE POST SIGN
---	CONTROL POINT	⊗	MAIL BOX
---	DRILL HOLE	⊕	BOLLARD
---	UTILITY POLE	⊗	CONTROLLER CABINET
---	UTILITY POLE WITH LIGHT	⊕	GAS GATE
---	TRAFFIC SIGNAL POLE	⊗	TELEPHONE BOX
---	ELECTRIC BOX	⊕	CATV TUBE
---	WETLAND FLAG		

PROJECT NUMBER
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OR AS NOTED
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GRAPHIC SCALE

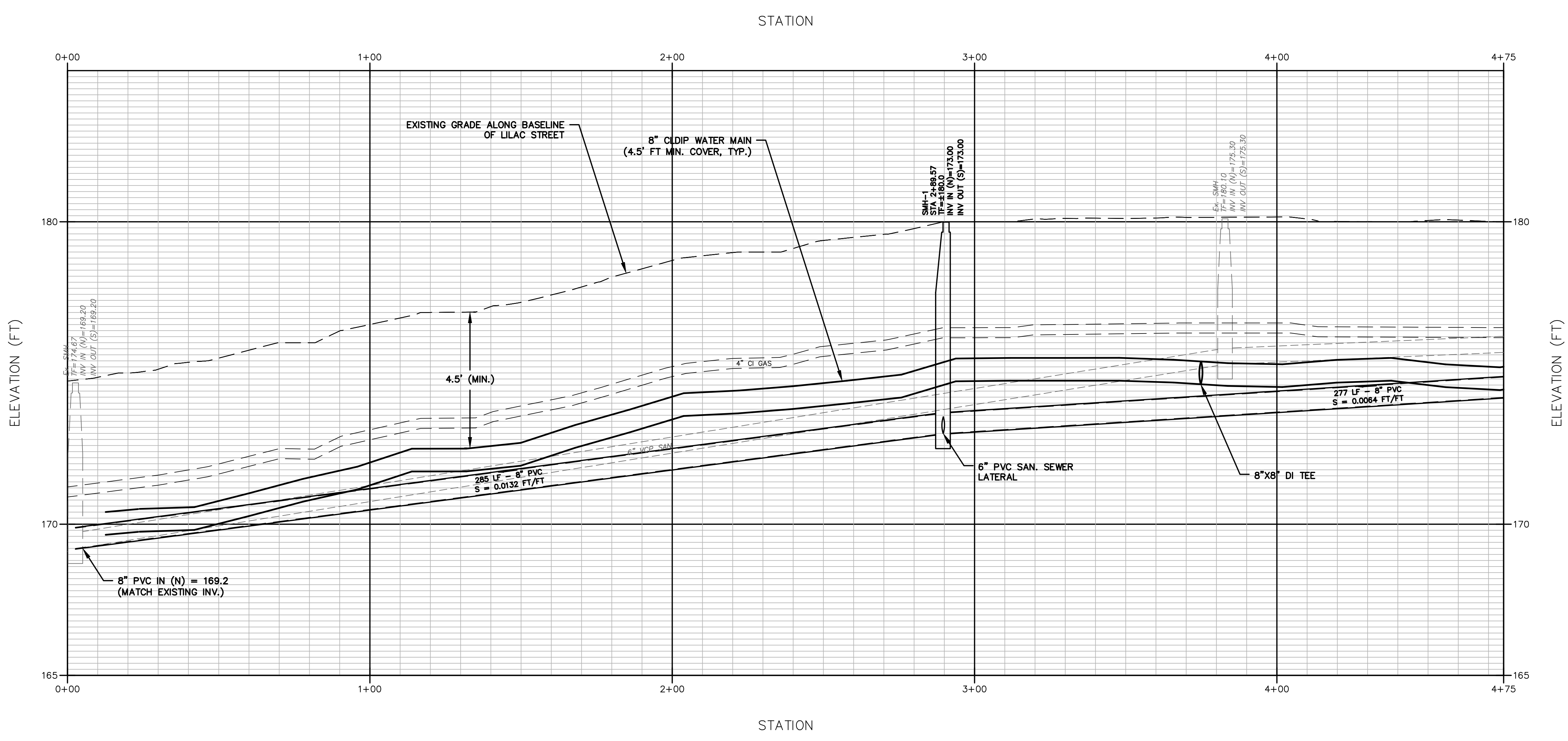
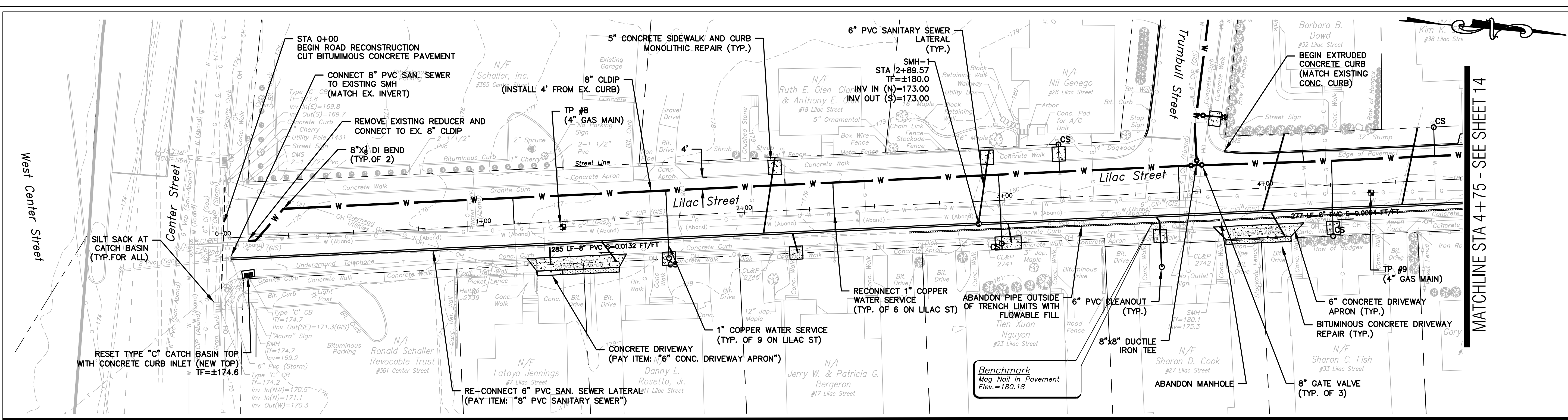
DATUM
HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION
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RIDGEWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.**

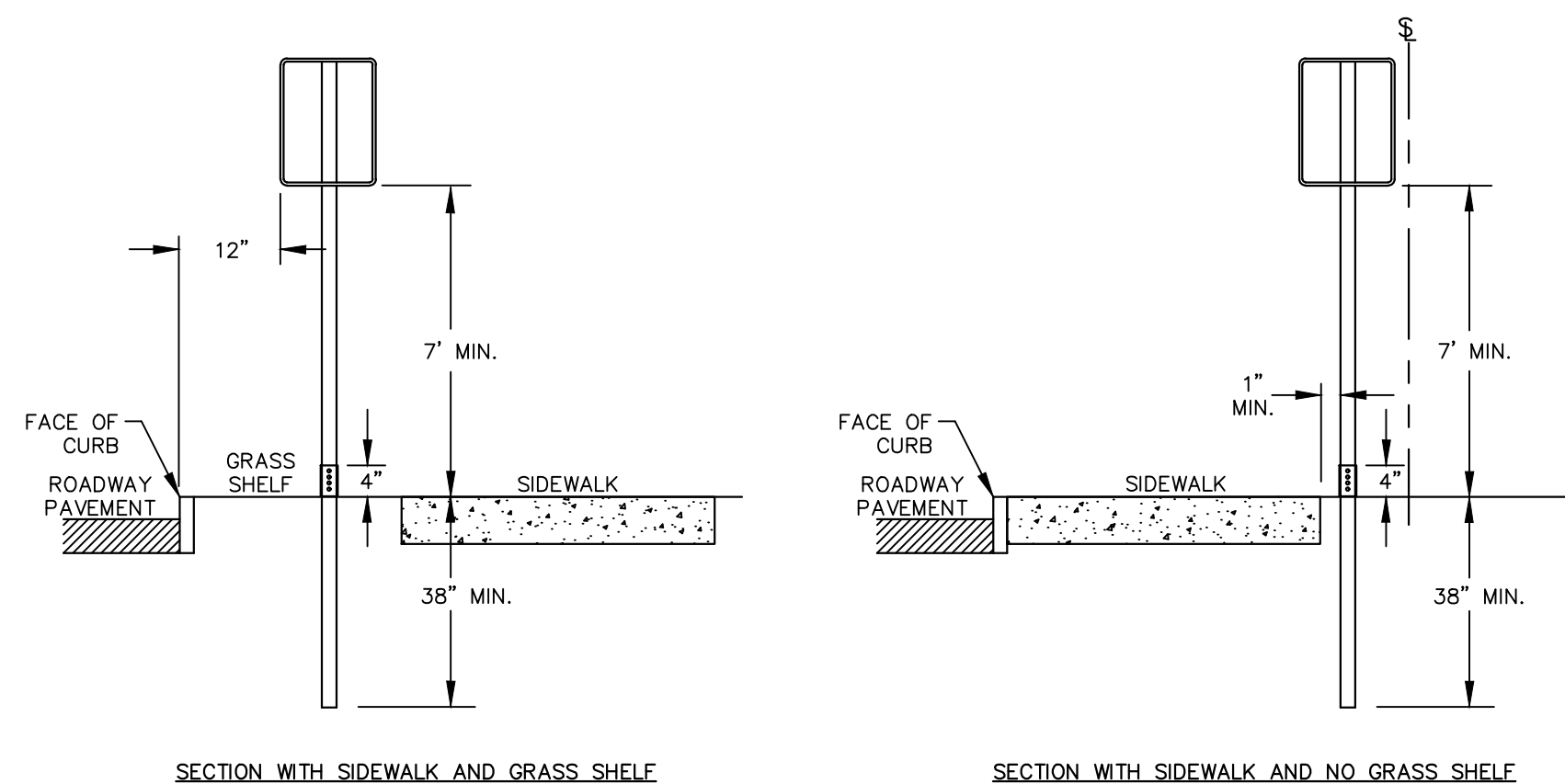
PROJECT TITLE
**RIDGEWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
**LILAC STREET
PLAN & PROFILE
(STA 0+00 TO STA 4+75)**

SHEET NUMBER
13 of 20

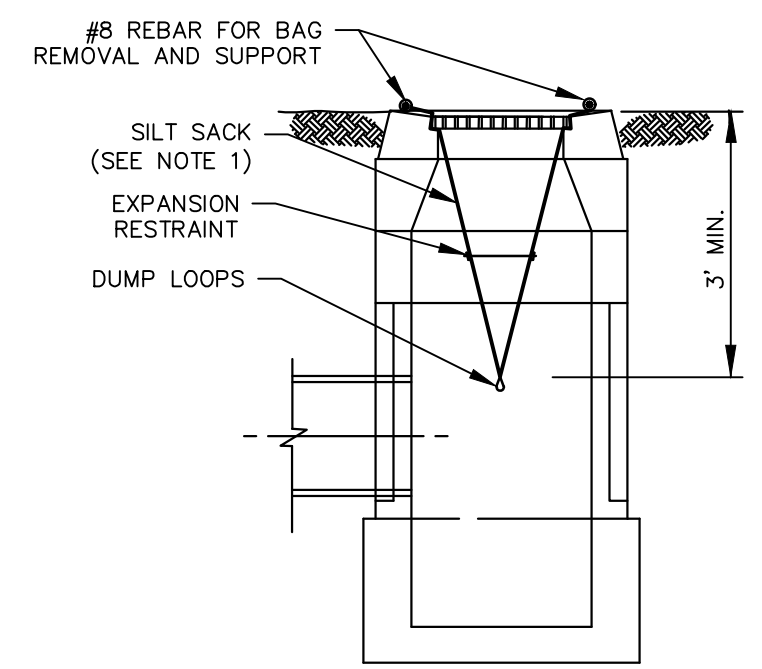


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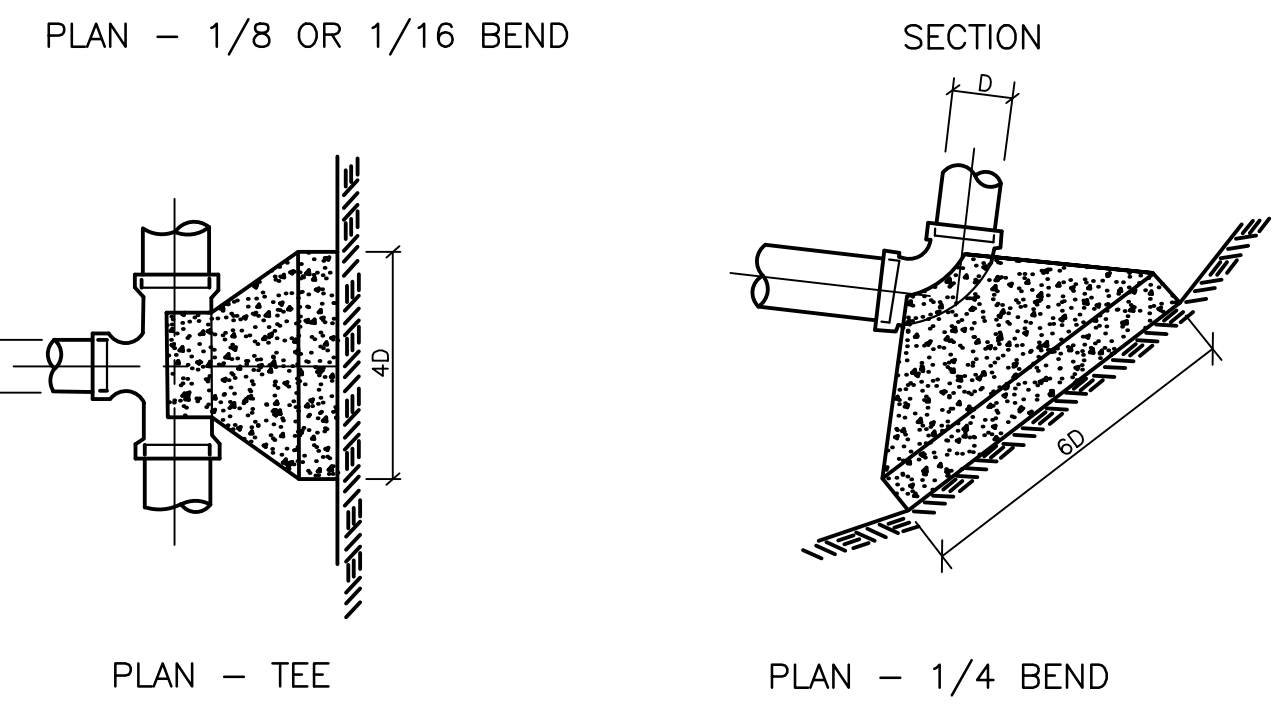
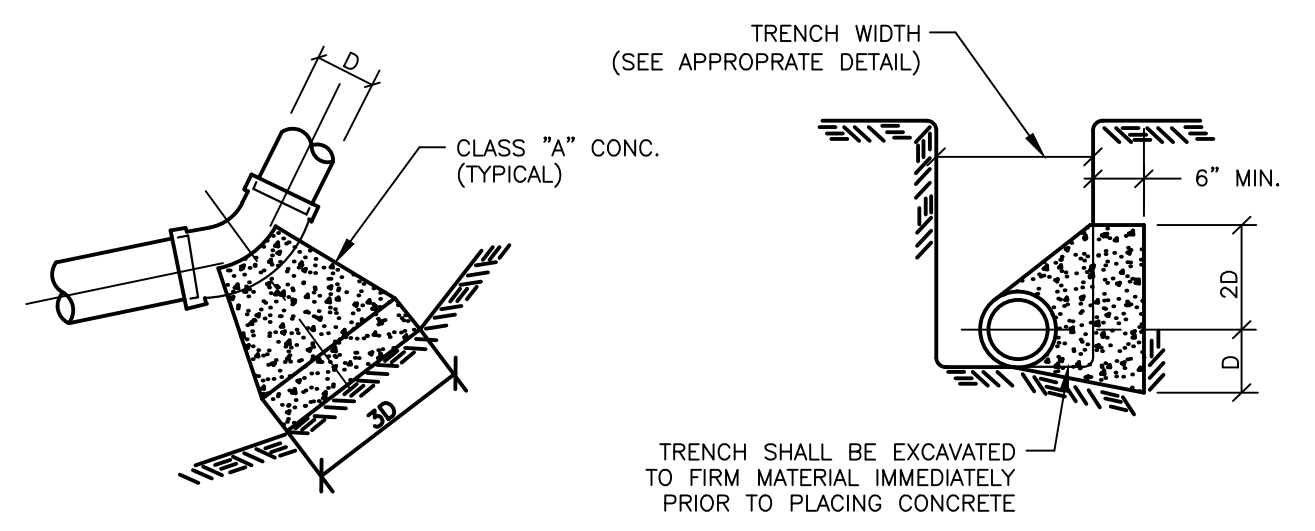
- NOTES:
- SUPPORTS SHALL BE METAL GALVANIZED STEEL POSTS WITH BREAKAWAY COUPLING SYSTEM.
 - WHERE POSTS CANNOT BE INSTALLED BEHIND SIDEWALK, THEY SHALL BE INSTALLED WITH THE EDGE OF THE SIGN 12" FROM FACE OF CURB.

SIGN INSTALLATION - TYPICAL
NOT TO SCALE



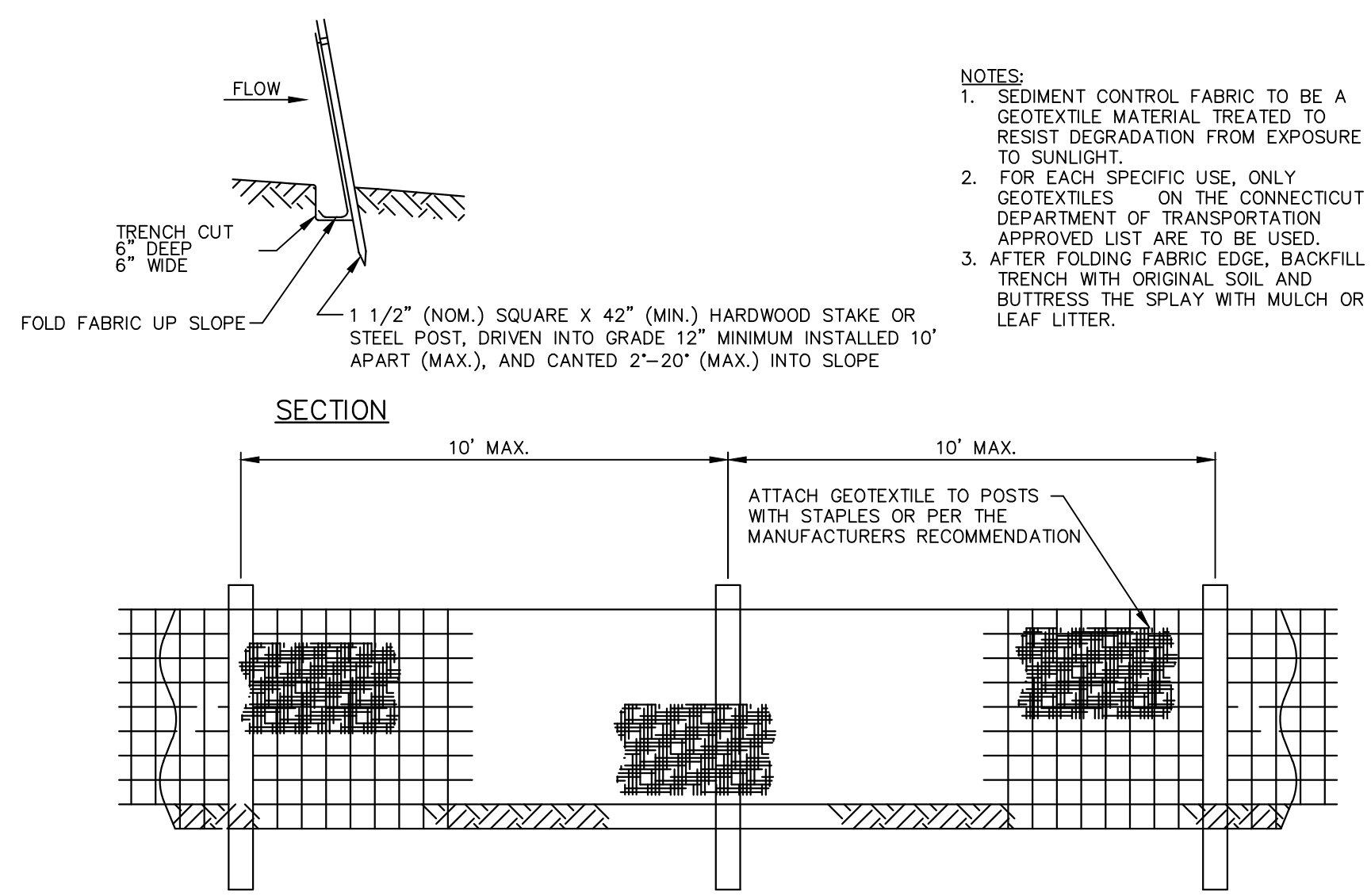
- NOTES:
- SILT SACKS SHALL BE HI-FLOW SILTSACK® "TYPE A" FOR TYPE "C-L" CB TOPS AND "TYPE B" WITH CURB DEFLECTORS FOR TYPE "C" CB TOPS OR OTHER STRUCTURES WITH CURB INLETS AS MANUFACTURED BY ACF ENVIRONMENTAL, INC. OR APPROVED EQUAL.
 - SILT SACKS SHALL BE PROVIDED WITH INTERNAL OVERFLOWS.
 - SILT SACKS SHALL BE EMPTIED WHEN THEY HAVE COLLECTED 6" TO 12" OF SEDIMENT, INSPECT EVERY 1 TO 2 WEEKS AND AFTER EVERY MAJOR RAINFALL EVENT.

SILT SACK
NOT TO SCALE



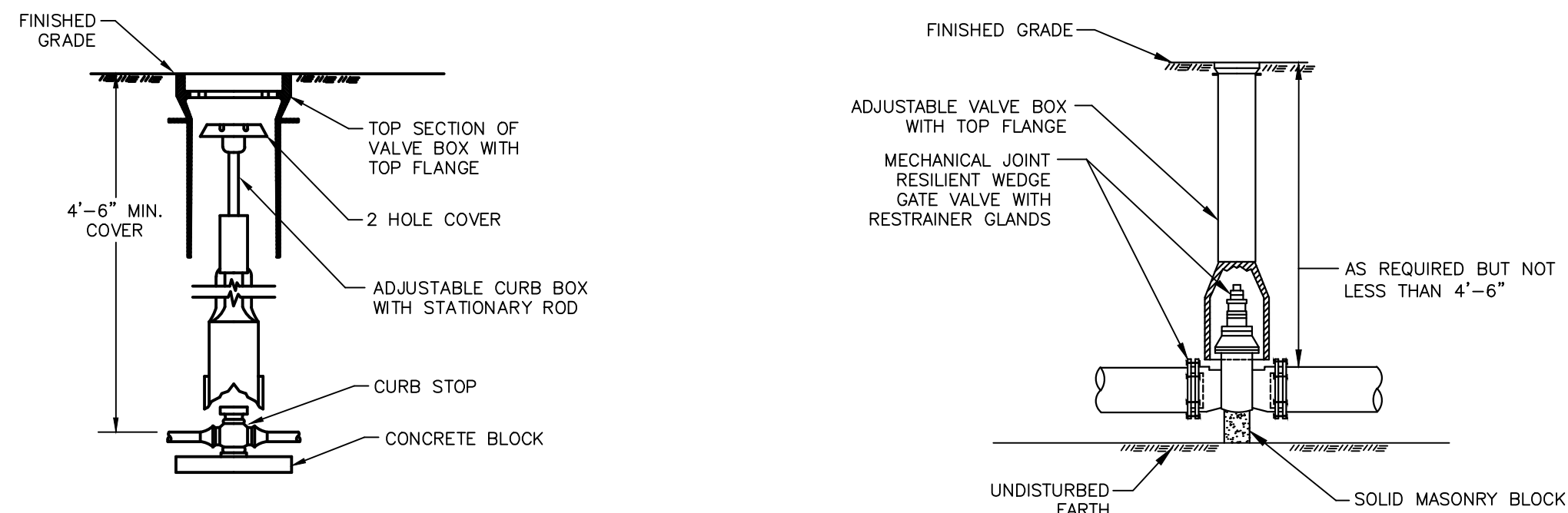
CONCRETE THRUST BLOCK
NOT TO SCALE

- NOTE:
- ALL JOINTS AT THRUST BLOCKS SHALL BE WRAPPED IN POLYETHYLENE ENCASMENT CONFORMING TO AWWA C105 OF LATEST REVISION.



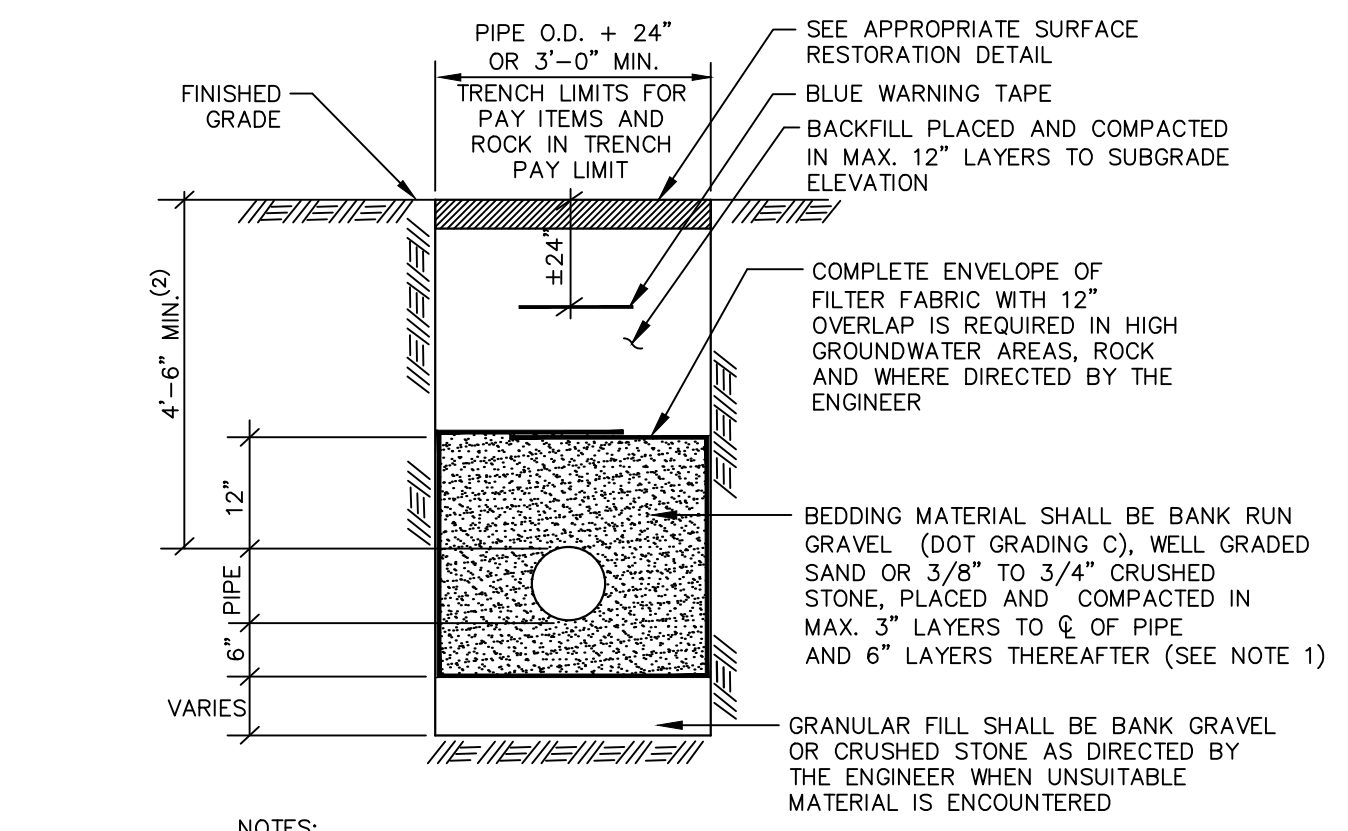
- NOTES:
- SEDIMENT CONTROL FABRIC TO BE A GEOTEXTILE MATERIAL TREATED TO RESIST DEGRADATION FROM EXPOSURE TO SUNLIGHT.
 - FOR EACH SPECIFIC USE, ONLY GEOTEXTILES ON THE CONNECTICUT DEPARTMENT OF TRANSPORTATION APPROVED LIST ARE TO BE USED.
 - AFTER FOLDING FABRIC EDGE, BACKFILL TRENCH WITH ORIGINAL SOIL AND BUTTRESS THE SPLAY WITH MULCH OR LEAF LITTER.

SILT FENCE
NOT TO SCALE



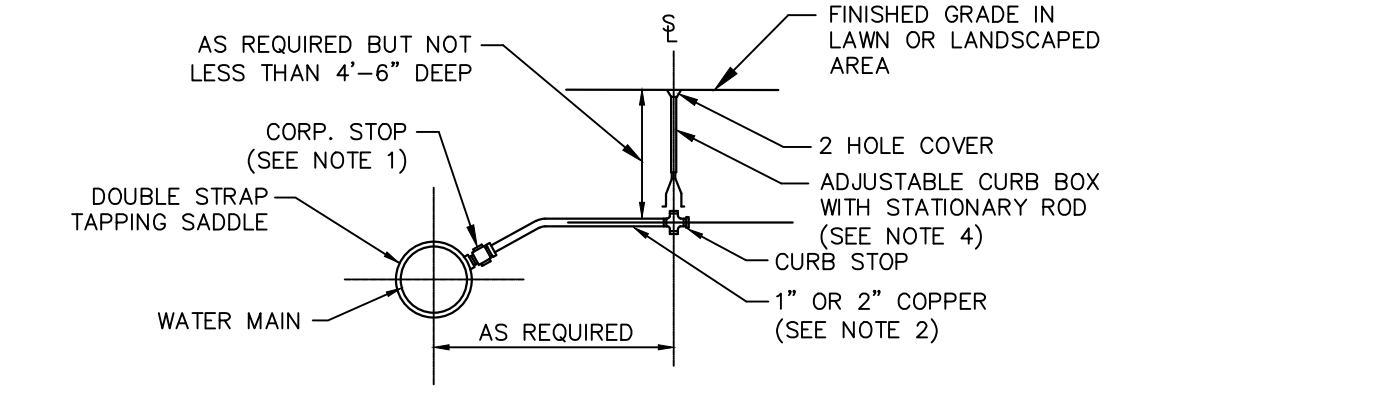
WATER SERVICE CURB STOP WITHIN PAVED AREAS AND SIDEWALK
NOT TO SCALE

WATER GATE VALVE
NOT TO SCALE



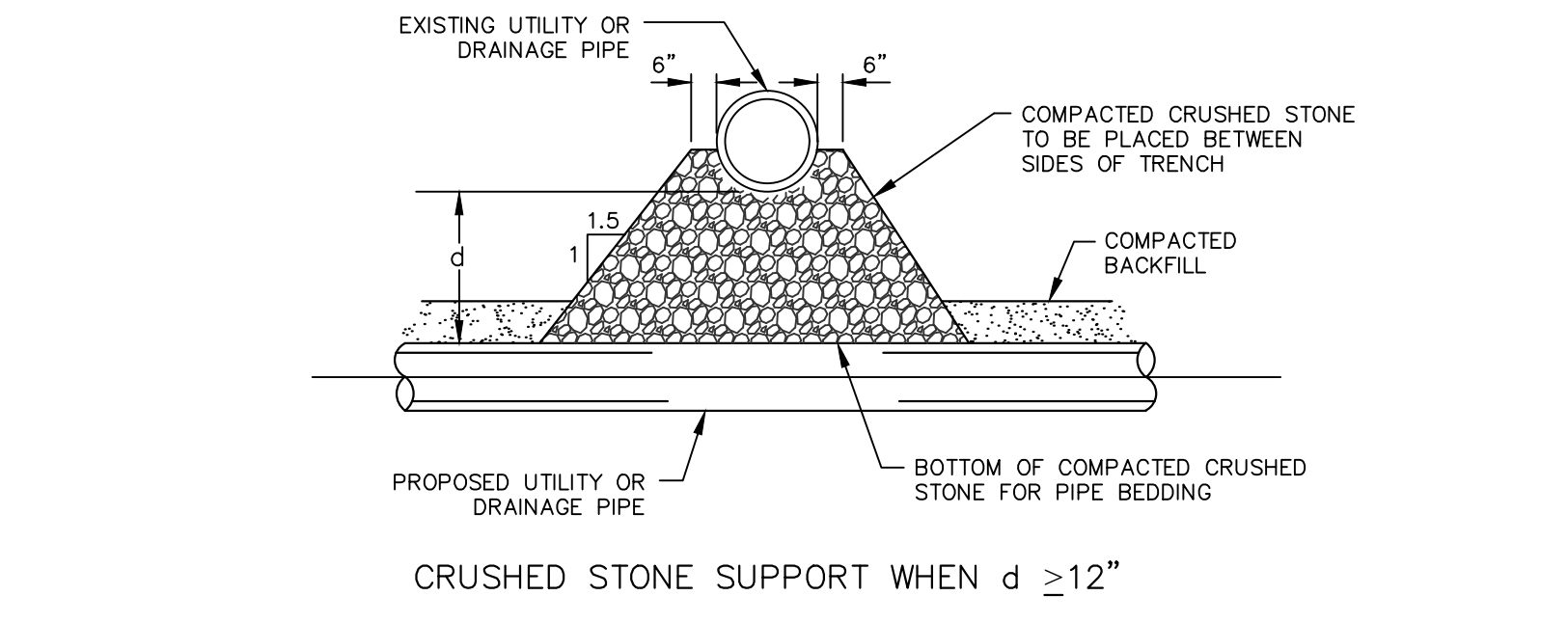
- NOTES:
- CRUSHED STONE SHALL ONLY BE USED IN HIGH GROUNDWATER CONDITIONS AS DIRECTED BY THE ENGINEER.
 - ALL WATER MAIN WITH LESS THAN 4'-6" OF COVER SHALL BE INSULATED UNLESS APPROVED OTHERWISE BY THE ENGINEER. SEE TYPICAL TRENCH DETAIL (INSULATED WATER) FOR ADDITIONAL INFORMATION.

TYPICAL TRENCH DETAIL (WATER)
NOT TO SCALE

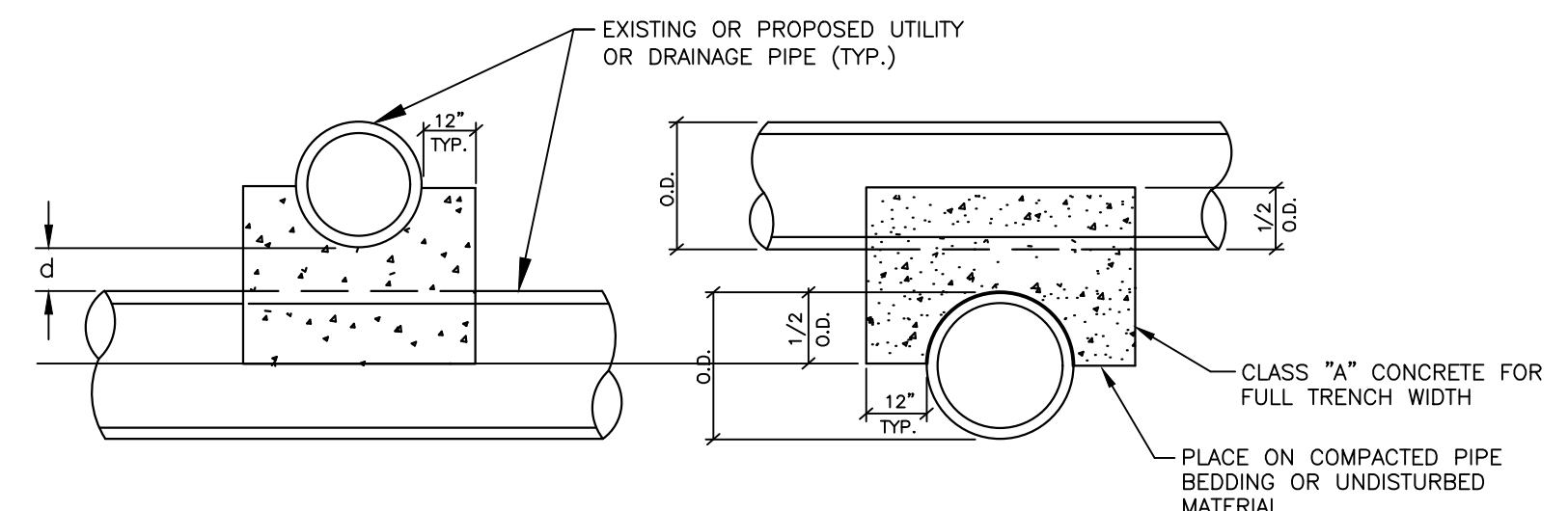


- NOTES:
- THE TOP OF THE CORPORATION AND THE FIRST THREE (3) FEET OF COPPER TUBING SHALL BE INSTALLED NO HIGHER THAN THE TOP OF THE WATER MAIN.
 - NO INTERMEDIATE SIZES (i.e. 3/4", 1 1/2", 1 3/4") ARE ALLOWED FOR COPPER SERVICES. ANY SERVICE REQUIREMENT GREATER THAN 2" COPPER SHALL BE CLDIP (4" MIN.) WITH THE SHUT-OFF LOCATED AT THE MAIN. COPPER TUBING SHALL BE CONTINUOUS WITH NO COUPLINGS BETWEEN THE CORPORATION STOP AND THE CURB STOP.
 - IN GENERAL, ALL EXISTING SERVICES THAT ARE CONSTRUCTED OF MATERIALS OTHER THAN COPPER TUBING BETWEEN THE CURB STOP AND METER SHALL BE REPLACED UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - TOP SECTION OF VALVE BOX WITH FLANGE SHALL BE SET AT FINISHED GRADE OVER CURB STOP COVER WHEN LOCATED WITHIN PAVED AREAS AND SIDEWALK.

TYPICAL WATER SERVICE CONNECTION
NOT TO SCALE



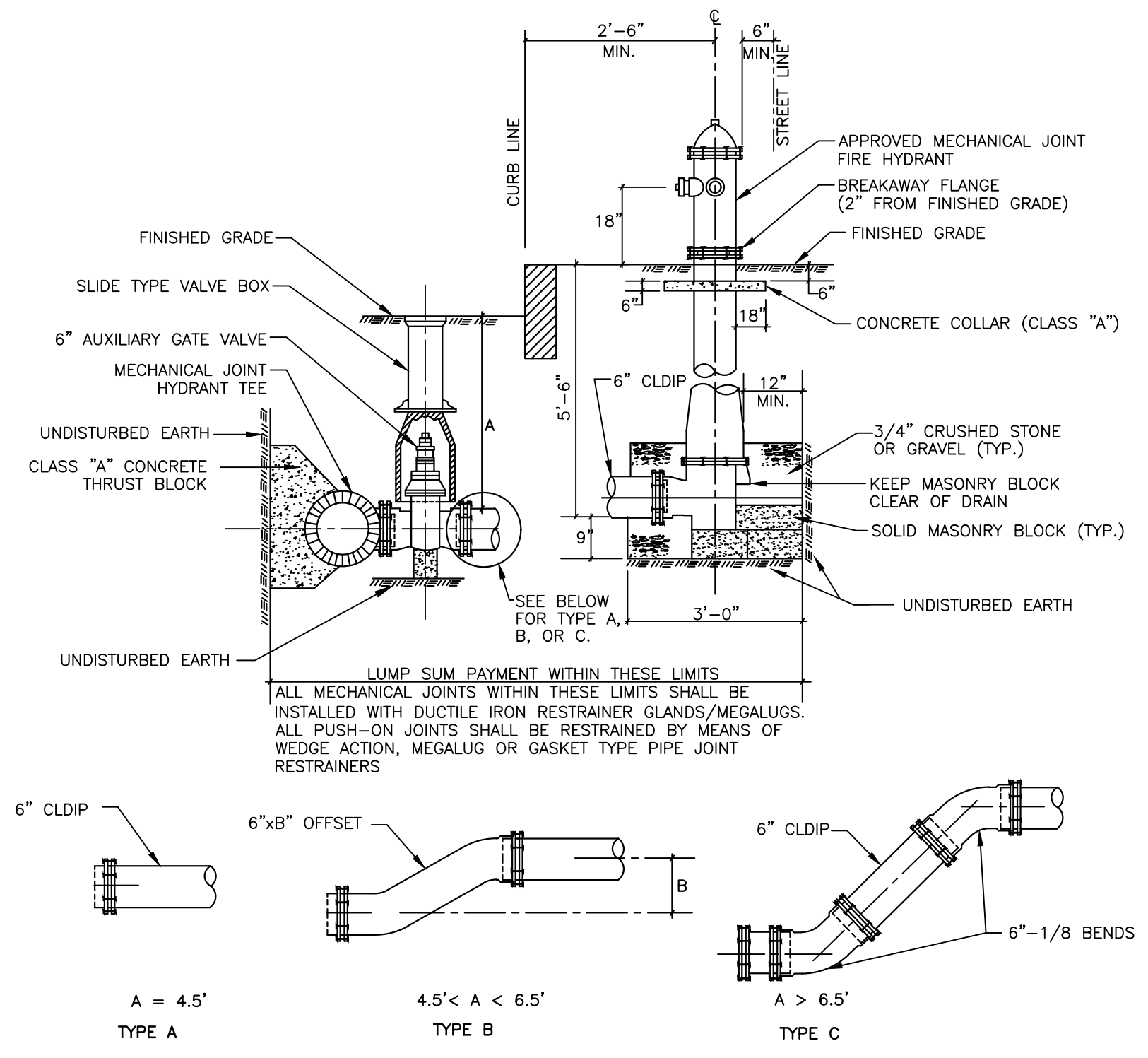
CRUSHED STONE SUPPORT WHEN $d \geq 12"$



CONCRETE PIPE CRADLE WHEN $d < 12"$

- NOTES:
- d = DISTANCE BETWEEN UTILITY AND DRAINAGE PIPES.
 - SUPPORTS SHALL BE INSTALLED WHERE SPECIFIED ON THE PLANS AND WHERE DIRECTED BY THE ENGINEER.
 - CRUSHED STONE SUPPORTS SHALL BE INCLUDED IN THE COST OF THE PROPOSED UTILITY OR DRAINAGE PIPE AND CONCRETE PIPE CRADLES SHALL BE PAID FOR AS "MISCELLANEOUS CONCRETE".

TYPICAL UTILITY SUPPORTS
NOT TO SCALE



- NOTES:
- TYPE "A" HYDRANT HAS NO OFFSETS OR BENDS BETWEEN THE AUXILIARY VALVE AND THE HYDRANT.
 - THE REQUIRED TYPE SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.

HYDRANT ASSEMBLY
NOT TO SCALE



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494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

---	WETLAND BOUNDARY	☆	LIGHT POLE
---	RETAINING WALL	⊗	CONIFEROUS TREE
---	GUIDE RAIL	⊙	DECIDUOUS TREE
---	STONE WALL	⊕	SANITARY MANHOLE
---	STOCKADE FENCE	⊖	DRAINAGE MANHOLE
---	WIRE FENCE	⊗	CATCH BASIN
---	CHAIN LINK FENCE	⊕	COLLECTOR END
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---	RAILROAD TRACKS	⊗	CURB STOP
---	SILT FENCE	⊕	WATER VALVE
---	CONCRETE MONUMENT	⊖	BUTTERFLY VALVE
---	GRANITE MONUMENT	⊗	BLOW OFF
---	IRON PIPE	⊕	SILO
---	IRON ROD	⊖	DOUBLE POST SIGN
---	CONTROL POINT	⊗	MAIL BOX
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---	UTILITY POLE	⊖	CONTROLLER CABINET
---	TRAFFIC SPAN POLE	⊗	GAS GATE
---	ELECTRIC BOX	⊕	TELEPHONE BOX
---	WETLAND FLAG	⊖	CATV TUBE

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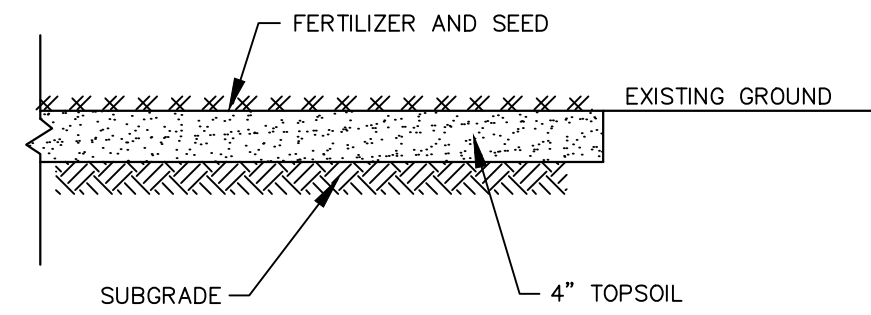
PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
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& LINCOLN ST,
MANCHESTER CT.**

PROJECT TITLE
**RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
DETAILS

SHEET NUMBER
15 of 20

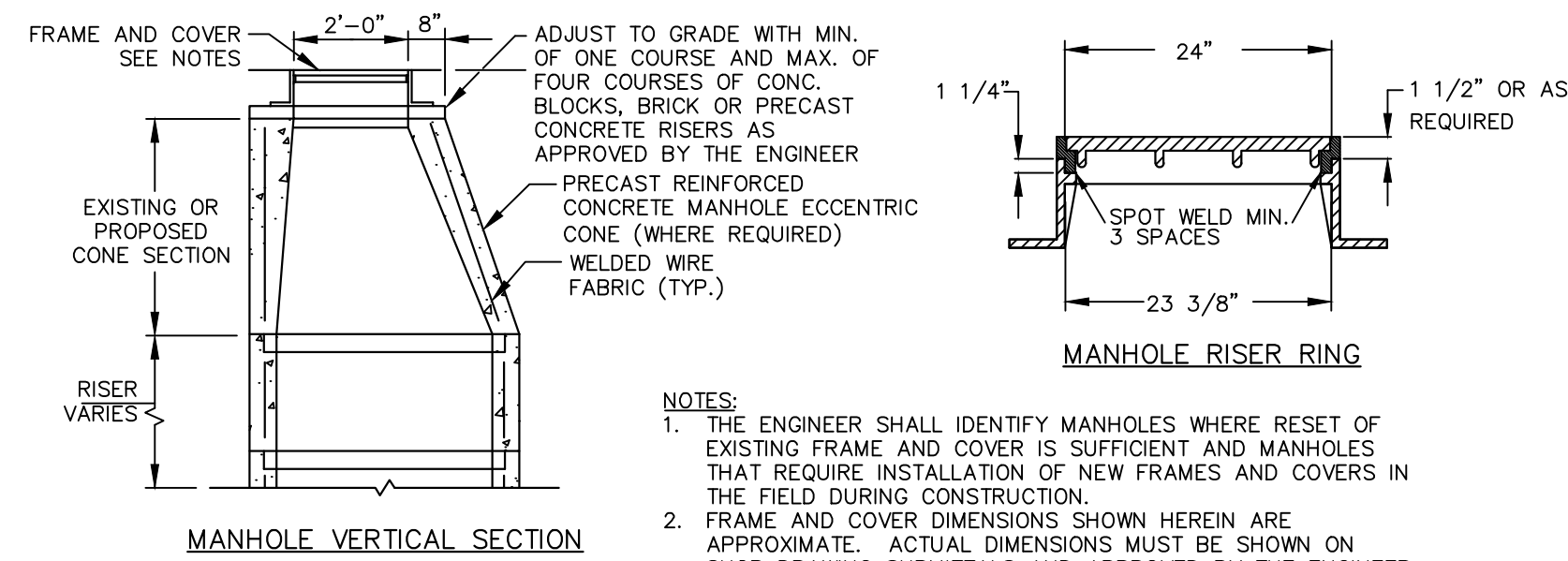
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- NOTES:
- REFER TO CONTRACT SPECIFICATIONS FOR SEED MIXTURES.
 - PERMANENT GRASS SEED MIXES SHALL BE APPLIED FROM APRIL 1 THROUGH JUNE 15 OR AUGUST 15 THROUGH OCTOBER 1.
 - PERENNIAL RYEGRASS SHALL BE APPLIED AS TEMPORARY GRASS SEED FROM MARCH 15 THROUGH JULY 1 OR AUGUST 1 THROUGH OCTOBER 15.

RESTORATION OF LAWN AREAS

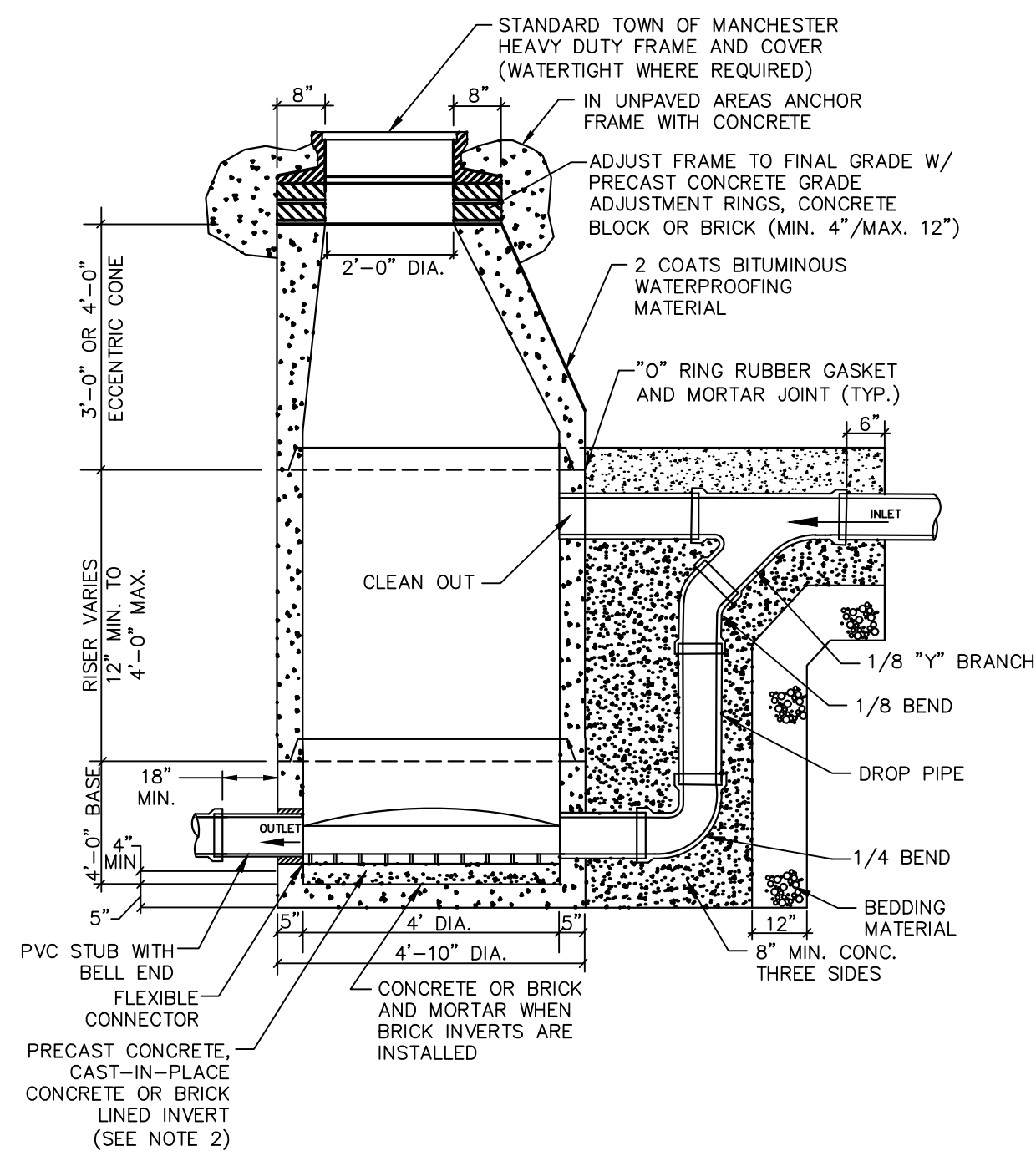
NOT TO SCALE



- NOTES:
- THE ENGINEER SHALL IDENTIFY MANHOLES WHERE RESET OF EXISTING FRAME AND COVER IS SUFFICIENT AND MANHOLES THAT REQUIRE INSTALLATION OF NEW FRAMES AND COVERS IN THE FIELD DURING CONSTRUCTION.
 - FRAME AND COVER DIMENSIONS SHOWN HEREIN ARE APPROXIMATE. ACTUAL DIMENSIONS MUST BE SHOWN ON SHOP DRAWING SUBMITTALS AND APPROVED BY THE ENGINEER.
 - ALL MANHOLE FRAMES COVERS SHALL BE TEMPORARILY SET FLUSH WITH THE BINDER COURSE OF PAVEMENT. A MANHOLE RISER RING SHALL BE INSTALLED TO RAISE MANHOLE COVER TO FINISHED GRADE PRIOR TO INSTALLATION OF THE FINAL SURFACE COURSE OF PAVEMENT AT NO ADDITIONAL COST TO THE TOWN.

RESET MANHOLE TO GRADE

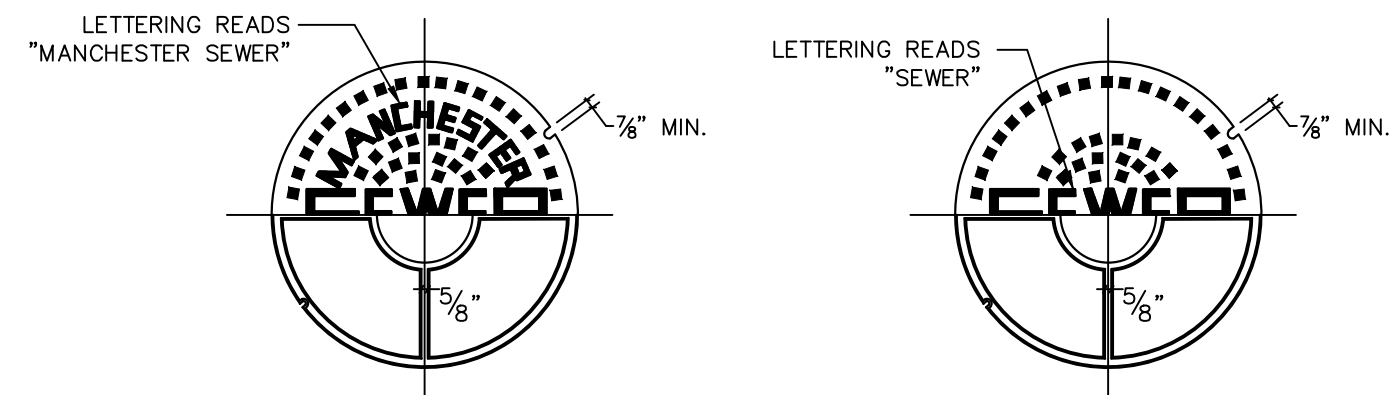
NOT TO SCALE



- NOTES:
- MAXIMUM PIPE SIZE TO BE USED IN 4" DIA. MANHOLE IS 24".
 - BRICK INVERT CONSTRUCTION IS SHOWN; HOWEVER, PRECAST OR CAST-IN-PLACE CONCRETE INVERTS SHALL BE INSTALLED UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - PROVIDE BEDDING MATERIAL IN ACCORDANCE WITH THE 48" SANITARY MANHOLE DETAIL.
 - FOR ALL OTHER REQUIREMENTS SEE 48" SANITARY MANHOLE DETAIL.

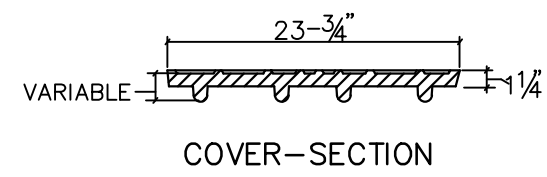
48" OUTSIDE DROP SANITARY MANHOLE

NOT TO SCALE

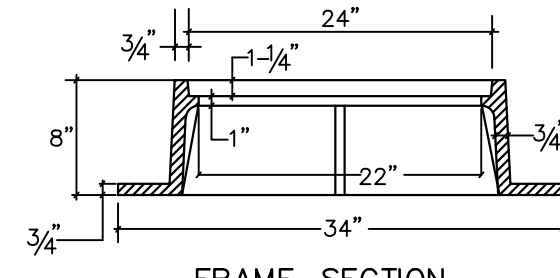


COVER-PLAN FOR SYSTEMS OWNED BY THE TOWN OF MANCHESTER

COVER-PLAN FOR PRIVATELY-OWNED SYSTEMS



COVER-SECTION

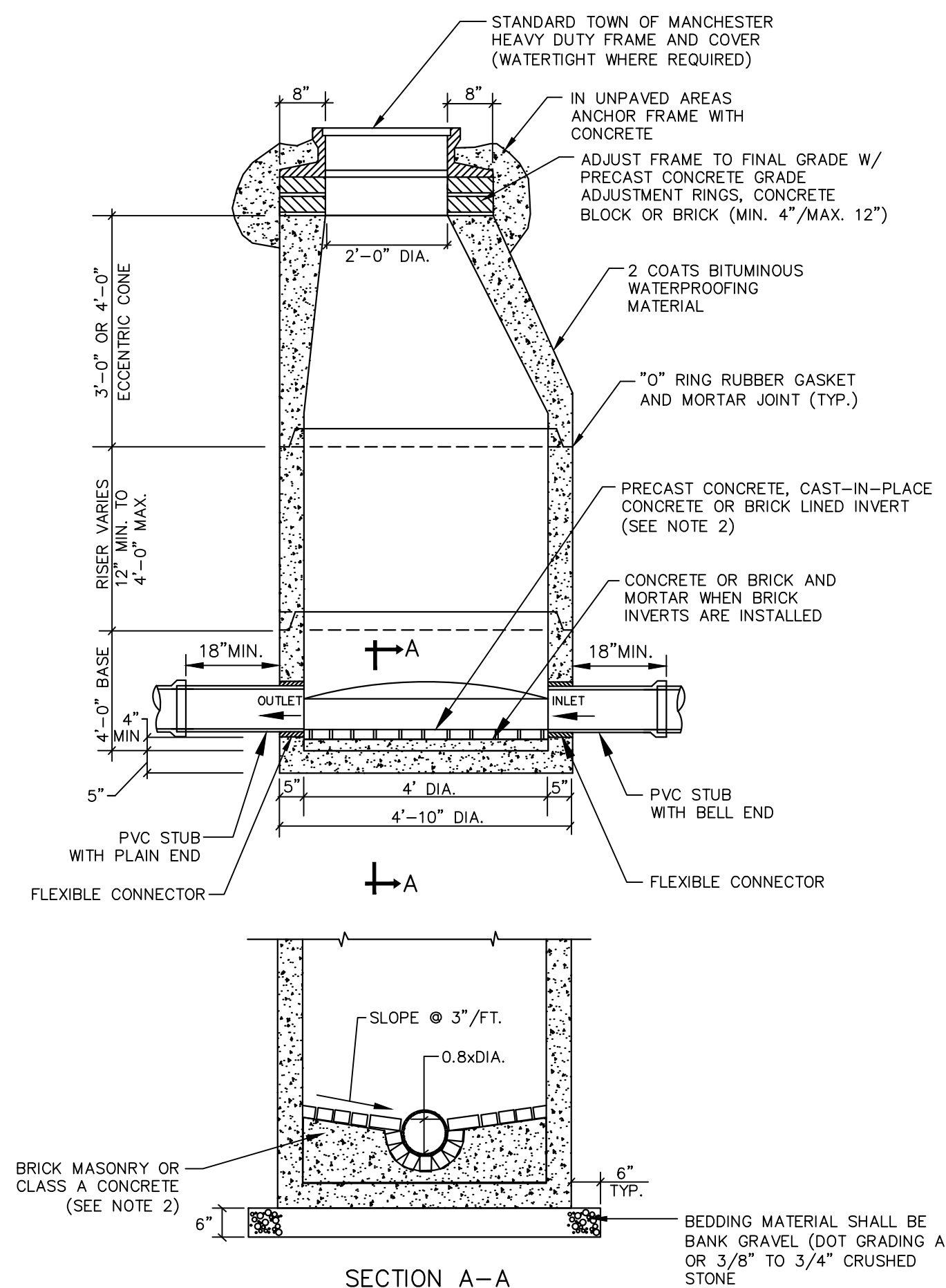


FRAME-SECTION

- NOTES:
- MANHOLE FRAMES AND COVERS SHALL BE THE MODEL AND MANUFACTURER LISTED IN THE CONTRACT SPECIFICATIONS.
 - BOLTS FOR BOLTED COVERS SHALL BE 1/2" STAINLESS STEEL.

SANITARY SEWER MANHOLE FRAME AND COVER

NOT TO SCALE

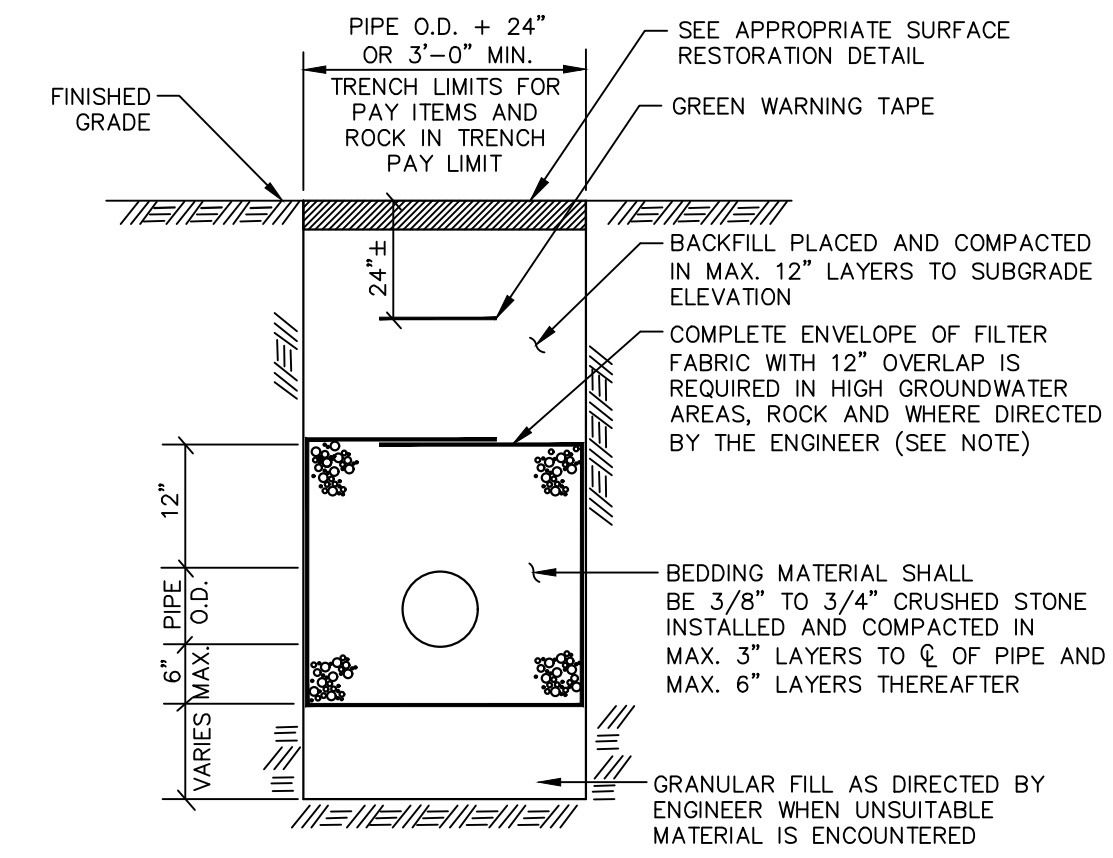


SECTION A-A

- NOTES:
- MAXIMUM PIPE SIZE TO BE USED IN 4" DIA. MANHOLE IS 24".
 - BRICK INVERT CONSTRUCTION IS SHOWN; HOWEVER, PRECAST OR CAST-IN-PLACE CONCRETE INVERTS SHALL BE INSTALLED UNLESS DIRECTED OTHERWISE BY THE ENGINEER.

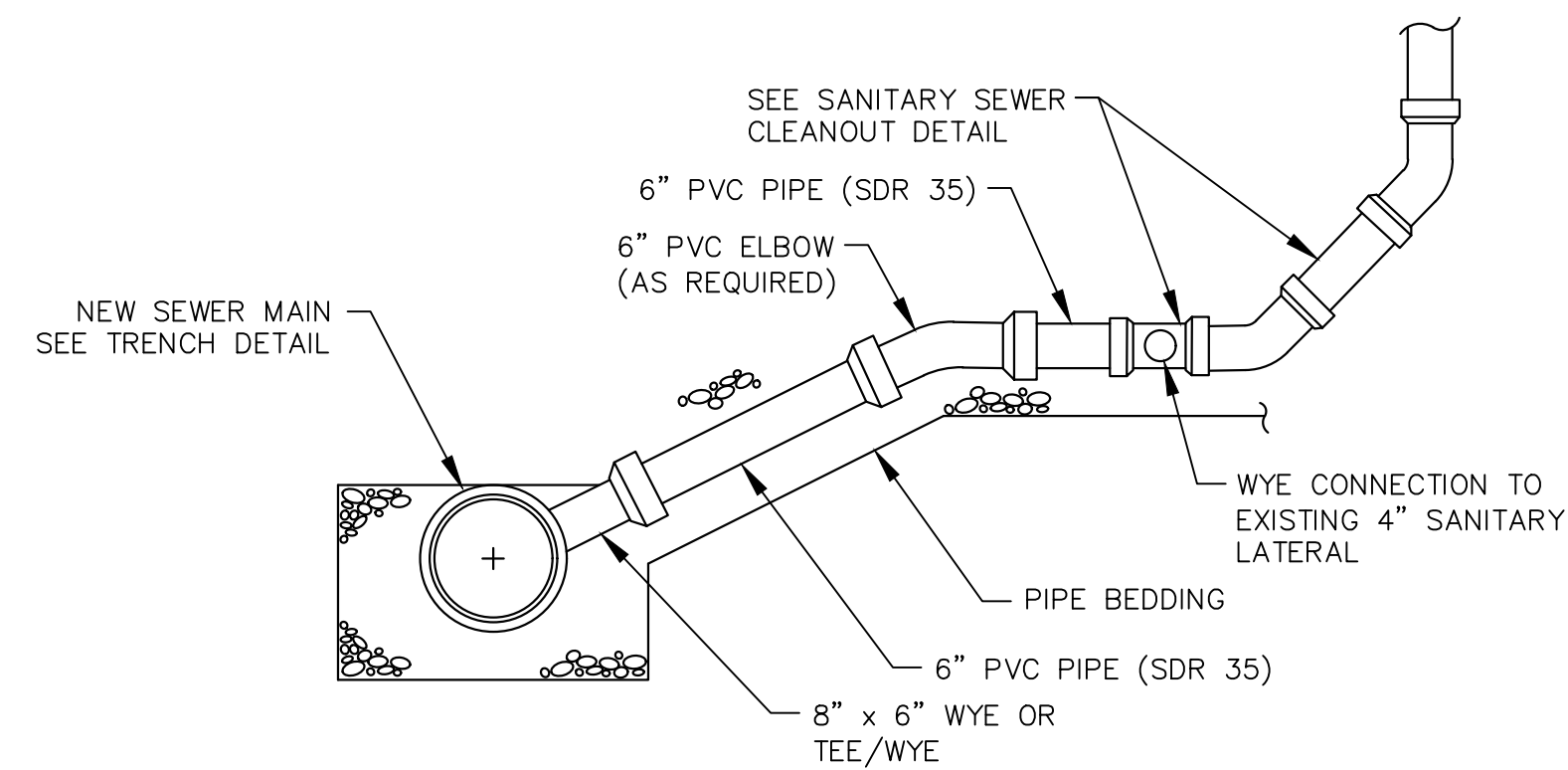
48" SANITARY MANHOLE

NOT TO SCALE



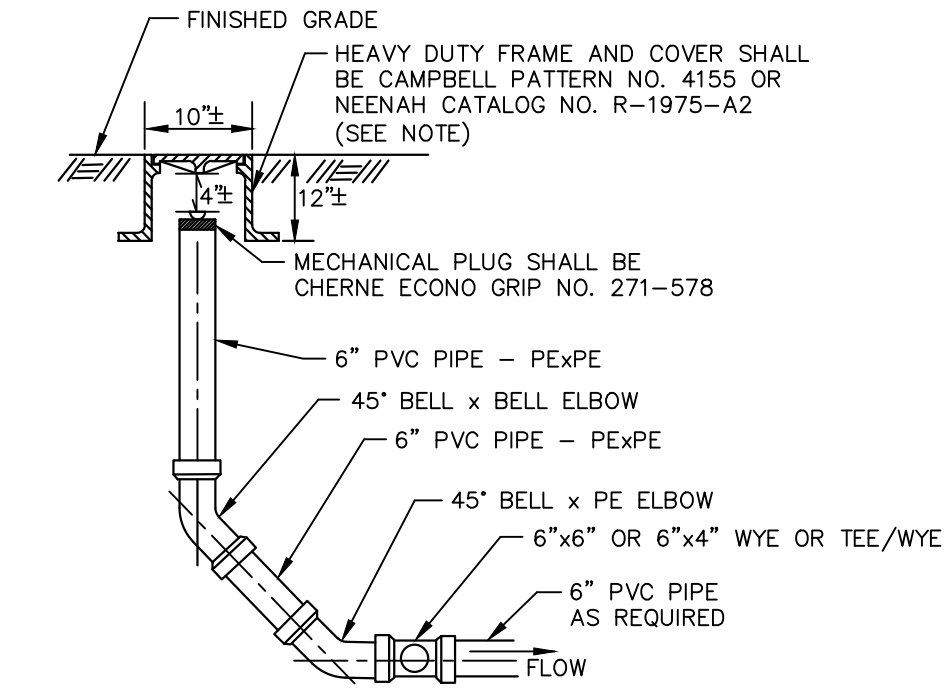
TYPICAL TRENCH DETAIL (SANITARY SEWER)

NOT TO SCALE



6" SANITARY LATERAL RECONNECTION (PAY ITEM: "6" PVC SANITARY SEWER LATERAL")

NOT TO SCALE



SANITARY SEWER CLEANOUT

NOT TO SCALE



TOWN OF MANCHESTER
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494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

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---	WETLAND FLAG	⊗	CATV TUBE

PROJECT NUMBER

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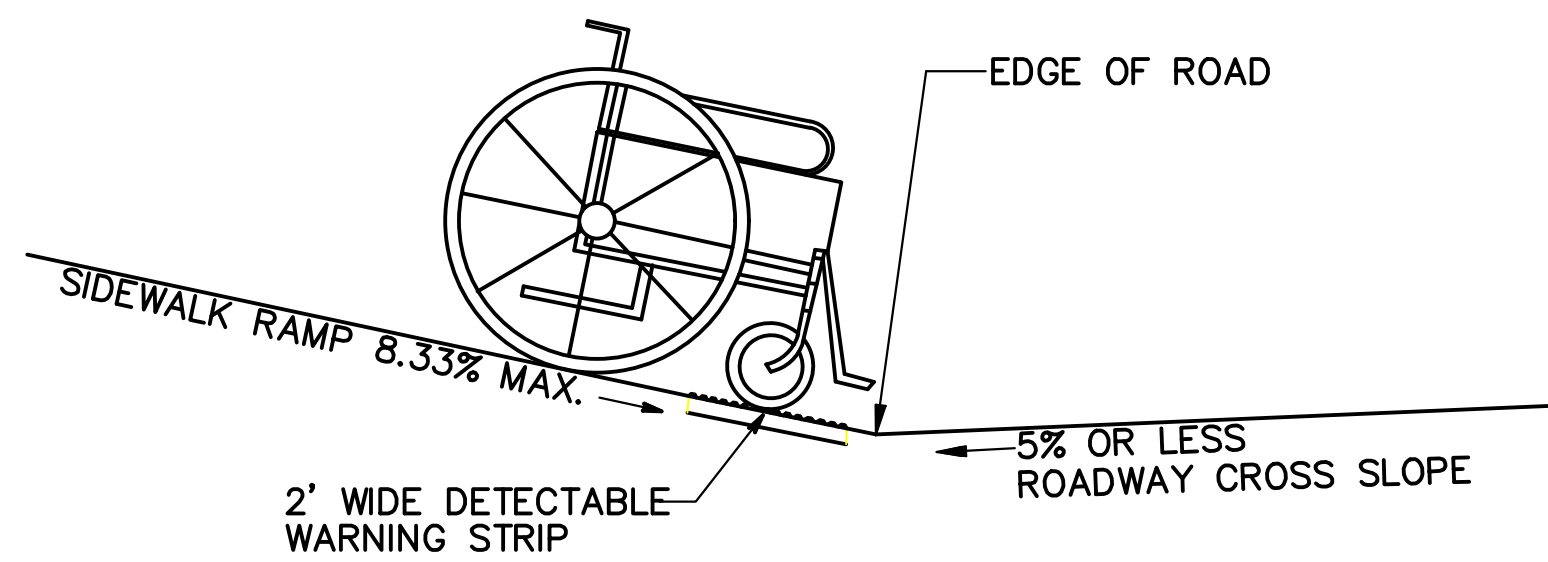
SHEET TITLE
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SHEET NUMBER
16 of 20

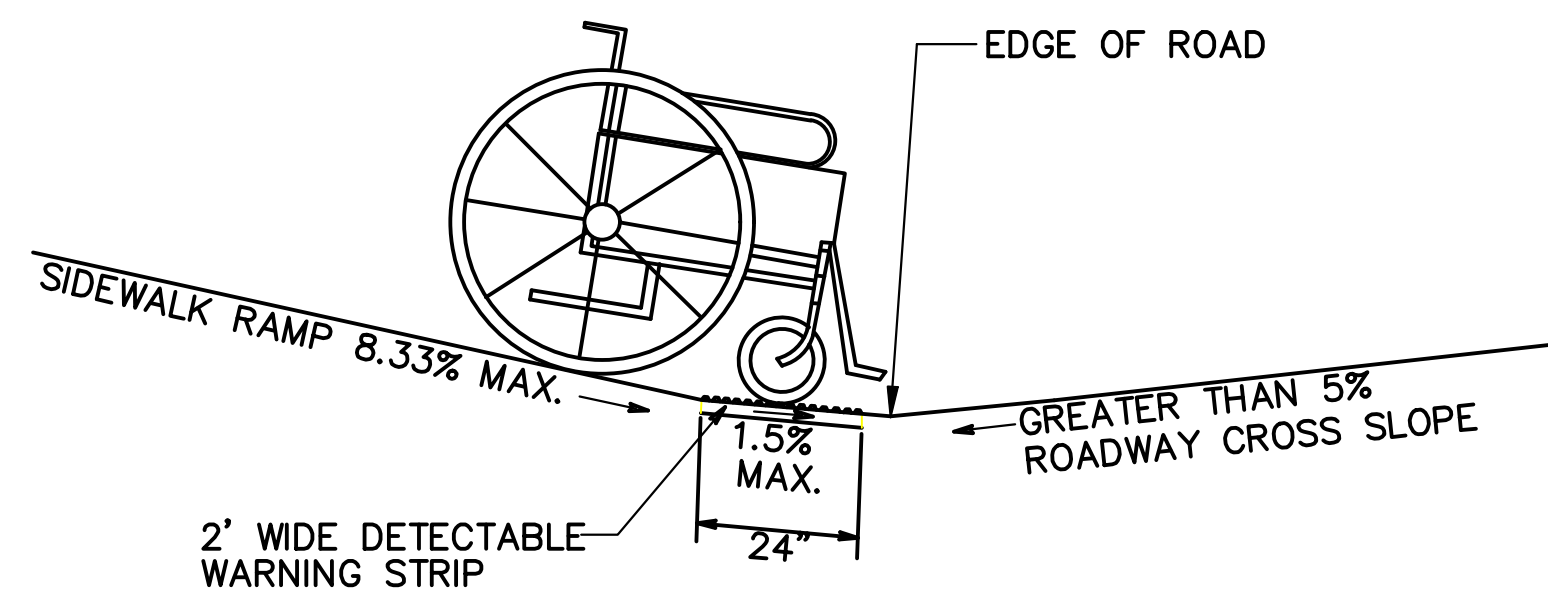
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GENERAL NOTES

1. SIDEWALK RAMPS SHALL HAVE A COARSE BROOM FINISH TRAVERSE TO THE SLOPE OF THE RAMP.
2. VERTICAL SURFACE DISCONTINUITIES AT JOINTS SHALL NOT EXCEED 1/4 INCH.
3. REMOVAL OF EXISTING SIDEWALK FOR NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION OR CONTRACTION JOINT.
4. THE RUNNING SLOPE OF THE CURB RAMP SHALL BE 8.3 PERCENT MAXIMUM BUT SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET.

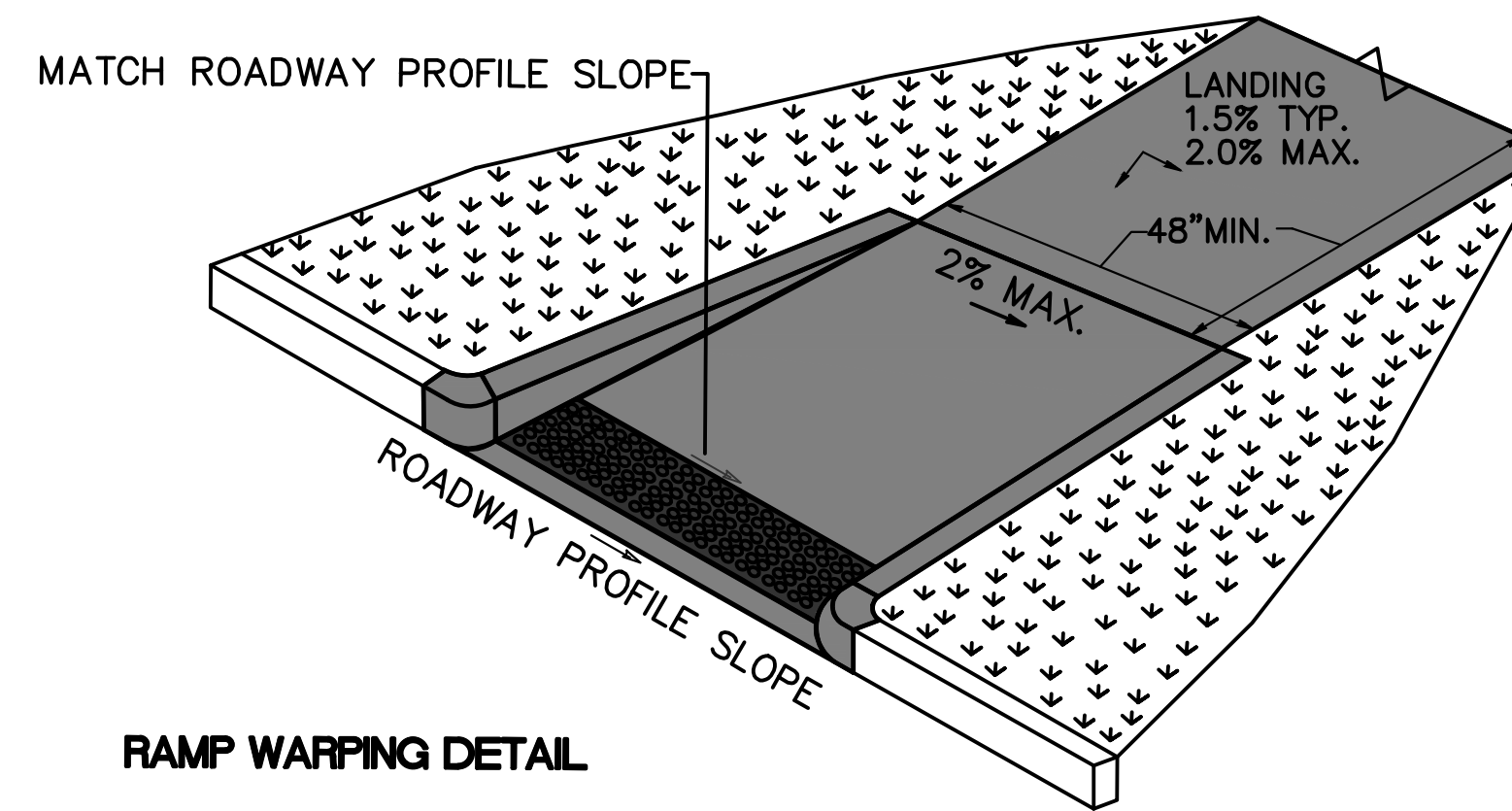


SIDEWALK RAMP GRADE AT ROADWAY CROSS SLOPE OF 5% OR LESS

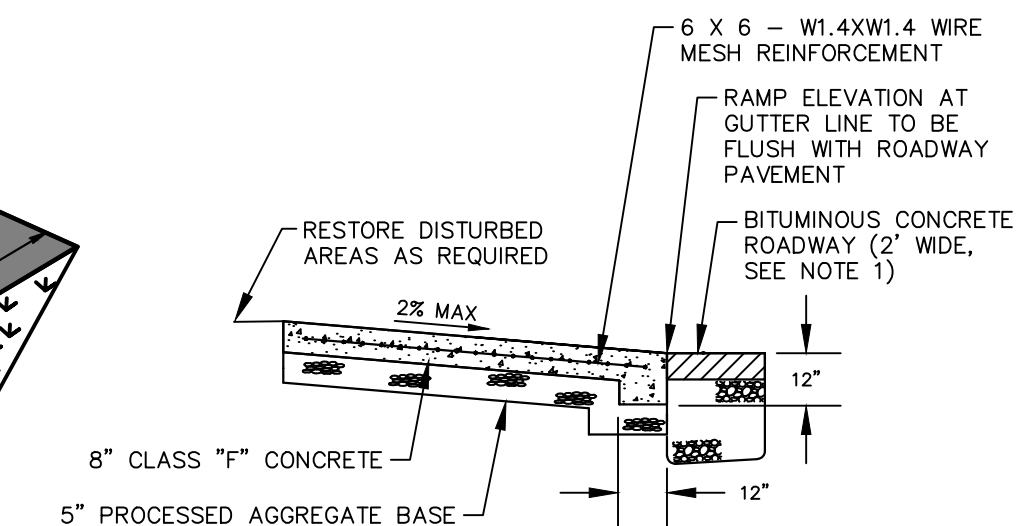


SIDEWALK RAMP GRADE AT ROADWAY CROSS SLOPE OF GREATER THAN 5%

RAMP WARPING DETAIL



1. TRANSITION SIDEWALK RAMP TO MATCH ROADWAY PROFILE AS GRADUALLY AS POSSIBLE. DO NOT EXCEED 3 % PER FOOT CROSS SLOPE RATE OF CHANGE WHEN TRANSITIONING TO ROADWAY PROFILE.
2. COMPLETE TRANSITION TO ROADWAY PROFILE BEHIND DETECTABLE WARNING SURFACE.



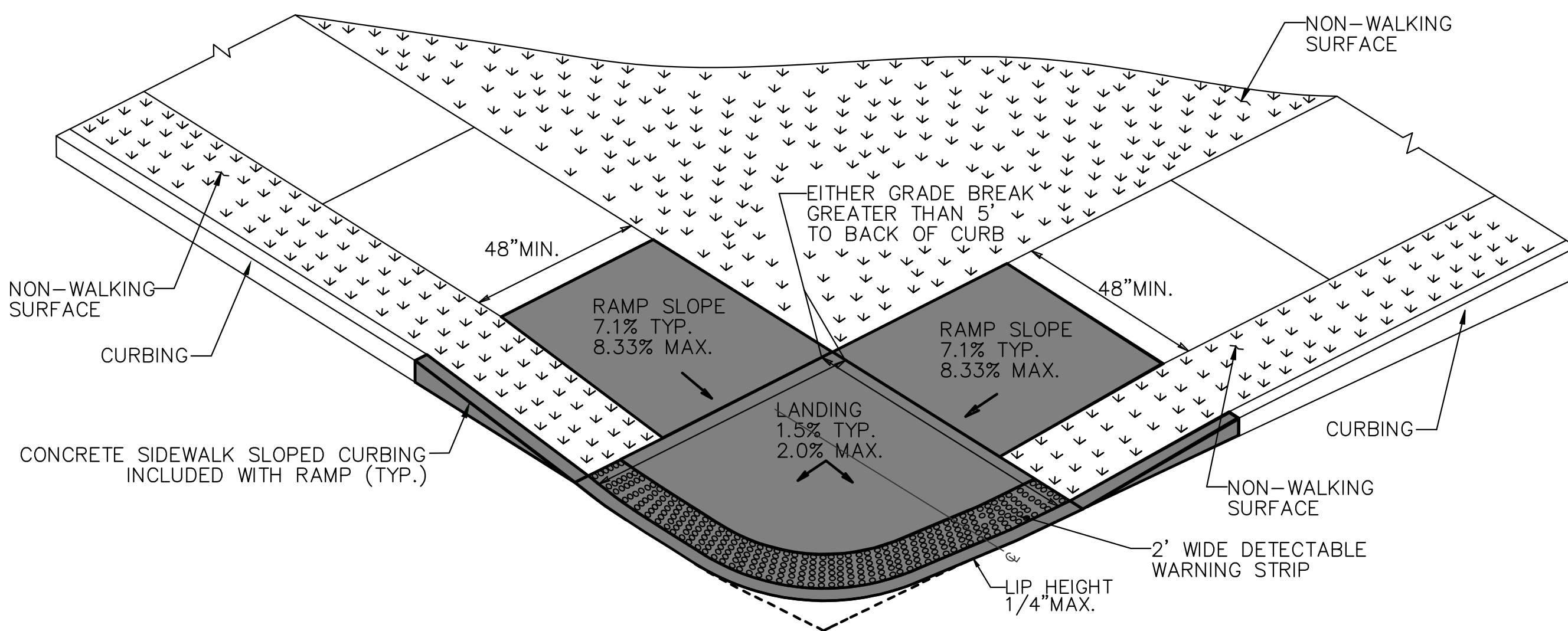
TYPICAL SECTION OF SIDEWALK LANDING AREA AT G

NOTES:

1. PAVEMENT COURSES AND PROCESSED AGGREGATE BASE THICKNESS SHALL MATCH EXISTING OR BE INSTALLED AS DIRECTED BY THE TOWN. PROCESSED AGGREGATE BASE THICKNESS SHALL BE 12\"/>
- 2. REFER TO CONCRETE SIDEWALK RAMP NOTES FOR ADDITIONAL INFORMATION.
- 3. IN GENERAL, GRANITE CURB TRANSITIONS ARE TO BE INSTALLED WHEN RAMP IS ADJACENT TO GRANITE CURB AND CONCRETE CURB TRANSITIONS ARE TO BE INSTALLED WHEN RAMP IS ADJACENT TO BITUMINOUS CONCRETE LIP CURB OR CONCRETE CURB. A MINIMUM LENGTH OF 3' SHALL BE USED FOR CURB TRANSITIONS.
- 4. IF GRANITE CURB TRANSITIONS ARE INSTALLED, THE CONCRETE RAMP LANDING MAY BE POURED TO THE REQUIRED RADIUS AS DIRECTED BY THE ENGINEER.
- 5. LANDING AREAS SHALL NOT EXCEED 2% CROSS-SLOPE AS MEASURED IN ANY DIRECTION.

SIDEWALK RAMP NOTES

(CTDOT GUIDE SHEETS/TOWN OF MANCHESTER)
2020 P.1.S.



CTDOT TYPE 4 - CONCRETE SIDEWALK RAMP

(BLENDED TRANSITION WITH GRADE BREAK TO BACK OF CURB GREATER THAN 5' AND NON-WALKING SURFACE)
NOT TO SCALE



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LEGEND

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—	GRANITE MONUMENT	⊕	BLOW OFF
—	IRON PIPE	⊖	SIGN
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—	ELECTRIC BOX	⊙	TELEPHONE BOX
—	WETLAND FLAG	⊕	CATV TUBE

PROJECT NUMBER

2020088

FILENAME

2020088PLAN.DWG

NO.	DATE	FILE
—	06/10/22	FOR BIDDING

DRAWN BY: BK

CHECKED BY: JED

RELEASED BY: JED

DATUM

HORIZONTAL: NAD83 VERTICAL: NAVD88

PROJECT LOCATION

ST. JOHN ST, ALEXANDER ST,
RIDGWOOD ST, ROOSEVELT ST,
& LINCOLN ST,
MANCHESTER CT.

PROJECT TITLE

RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS

SHEET TITLE

DETAILS

SHEET NUMBER

18 of 20

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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
ENGINEERING DIVISION
494 MAIN STREET - P.O. BOX 191
MANCHESTER, CT 06045-0191

LEGEND

—	WETLAND BOUNDARY	☆	LIGHT POLE
—	RETAINING WALL	⊗	CONIFEROUS TREE
—	GRADE RAIL	⊙	DECIDUOUS TREE
—	STONE WALL	⊕	SANITARY MANHOLE
—	STOCKADE FENCE	⊖	DRAINAGE MANHOLE
—	WIRE FENCE	⊗	CATCH BASIN
—	CHAIN LINK FENCE	⊕	VALVE END
—	PROPERTY LINE	⊖	HYDRANT
—	RAILROAD TRACKS	⊗	CURB STOP
□	CONCRETE MONUMENT	⊕	WATER VALVE
■	GRANITE MONUMENT	⊖	BUTTERFLY VALVE
○	IRON PIPE	⊗	BLOW OFF
●	IRON ROD	⊕	SILO
△	CONTROL POINT	⊖	DOUBLE POST SIGN
⊕	TRAFFIC SPAN POLE	⊗	MAIL BOX
⊖	UTILITY POLE	⊕	DRILL HOLE
⊗	UTILITY POLE WITH LIGHT	⊖	BOLLARD
⊕	ELECTRIC BOX	⊗	CONTROLLER CABINET
⊖	WETLAND FLAG	⊕	TELEPHONE BOX
⊗		⊖	CATV TUBE

PROJECT NUMBER
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PROJECT LOCATION
**ST. JOHN ST, ALEXANDER ST,
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& LINCOLN ST,
MANCHESTER CT.**

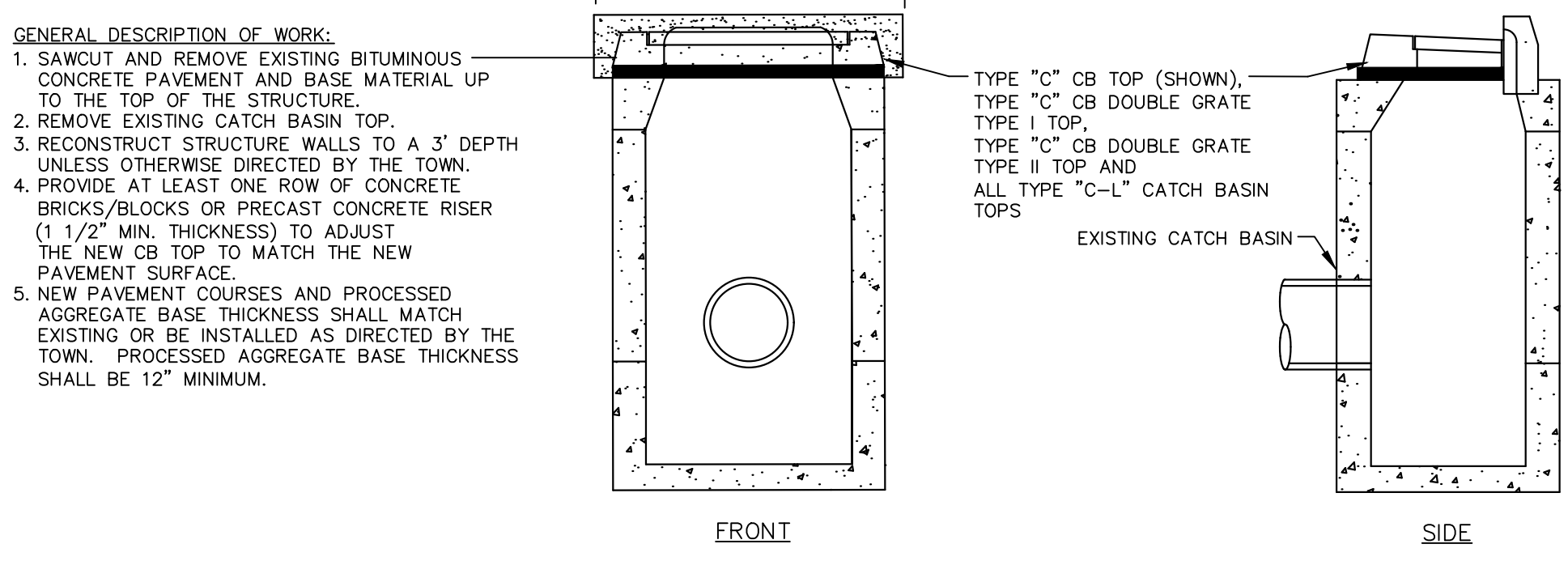
PROJECT TITLE
**RIDGWOOD STREET
-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE

DETAILS

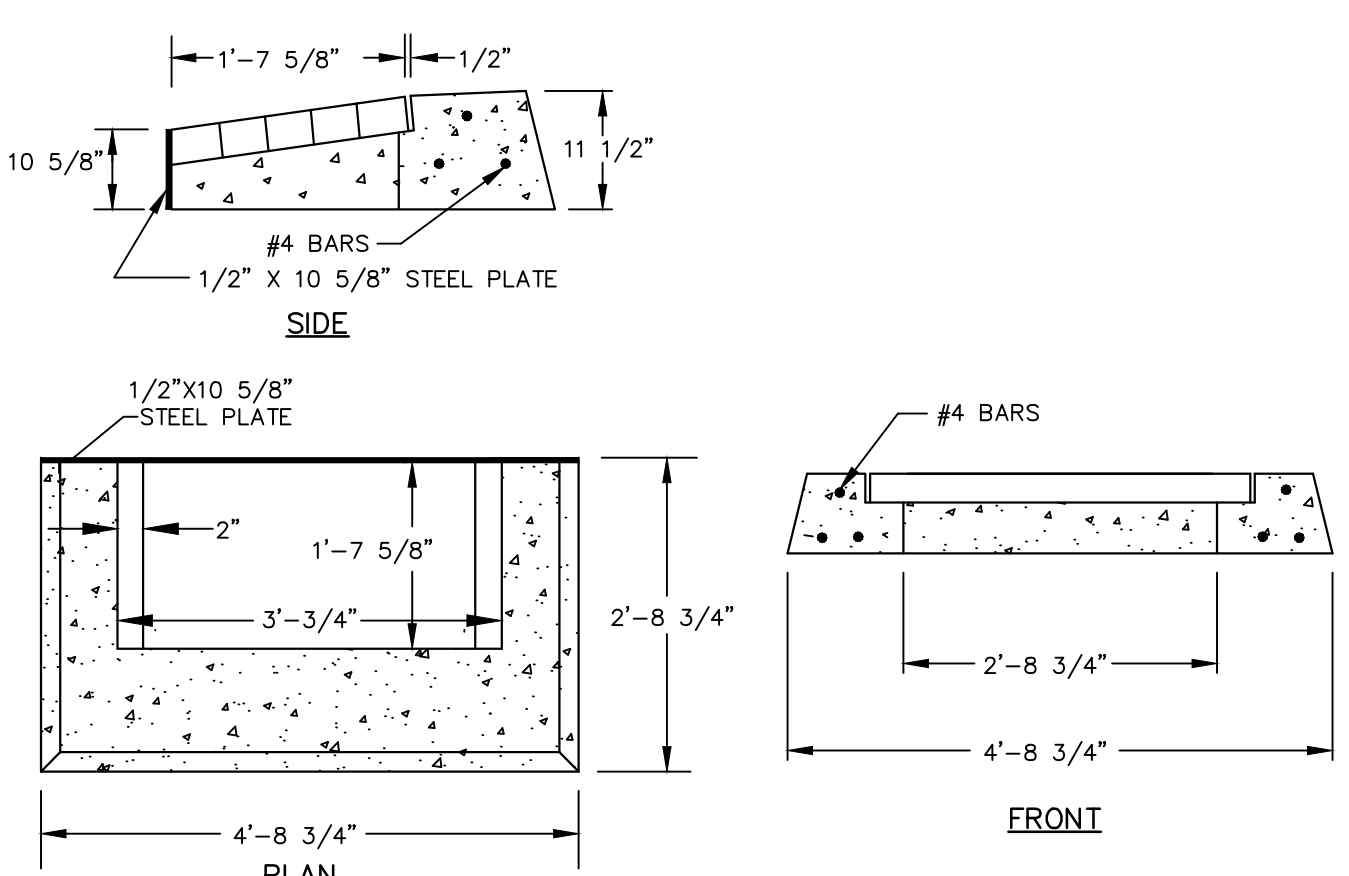
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19 of 20



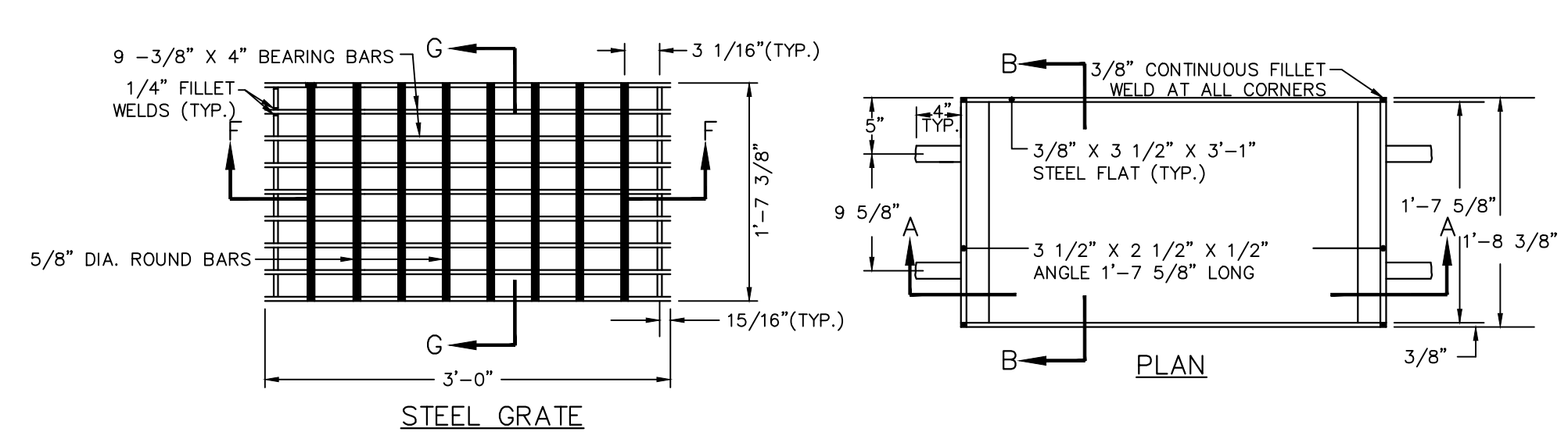
- GENERAL DESCRIPTION OF WORK:**
1. SAWCUT AND REMOVE EXISTING BITUMINOUS CONCRETE PAVEMENT AND BASE MATERIAL UP TO THE TOP OF THE STRUCTURE.
 2. REMOVE EXISTING CATCH BASIN TOP.
 3. RECONSTRUCT STRUCTURE WALLS TO A 3' DEPTH UNLESS OTHERWISE DIRECTED BY THE TOWN.
 4. PROVIDE AT LEAST ONE ROW OF CONCRETE BRICKS/BLOCKS OR PRECAST CONCRETE RISER (1 1/2" MIN. THICKNESS) TO ADJUST THE NEW CB TOP TO MATCH THE NEW PAVEMENT SURFACE.
 5. NEW PAVEMENT COURSES AND PROCESSED AGGREGATE BASE THICKNESS SHALL MATCH EXISTING OR BE INSTALLED AS DIRECTED BY THE TOWN. PROCESSED AGGREGATE BASE THICKNESS SHALL BE 12" MINIMUM.
- NOTES:**
1. WHEN CONCRETE BLOCKS ARE USED, MAXIMUM CORBEL IS 2" PER COURSE OF BLOCK.
 2. WALLS SHALL BE CONCRETE BLOCK OR PRECAST CONCRETE SECTIONS.
 3. WALL THICKNESS TO BE 12" WHEN TOTAL HEIGHT OF STRUCTURE EXCEEDS 10' FROM TOP OF FRAME TO BOTTOM OF BASE.
 4. THIS DETAIL SHOWN RESETTING A NEW TYPE "C" CATCH BASIN TOP WITH GRANITE CURB INLET; HOWEVER, IT SHALL BE USED FOR ALL CATCH BASIN TYPES SPECIFIED ON THE PLANS.

RESET CATCH BASIN TOP
NOT TO SCALE

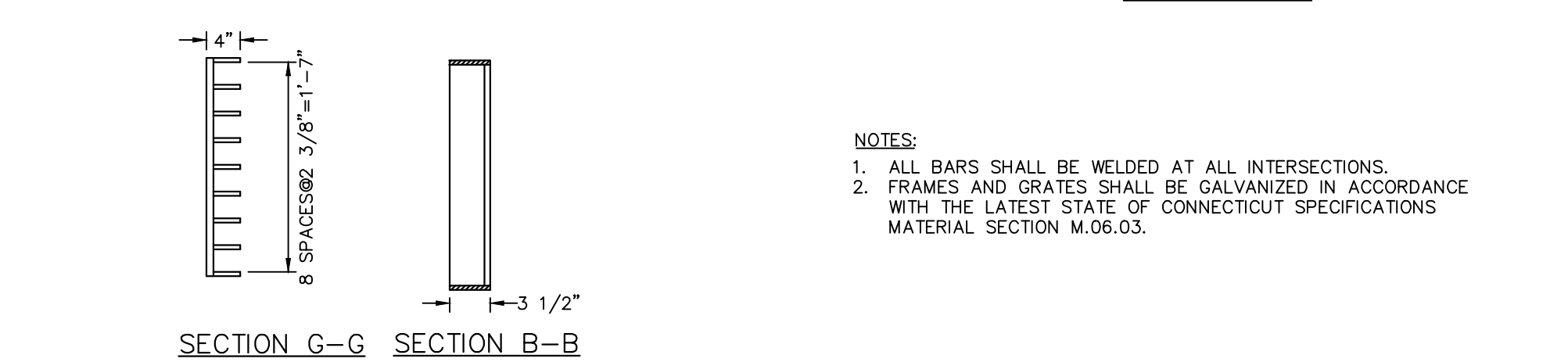


TYPE "C" CATCH BASIN TOP (3-SIDED)
NOT TO SCALE

NOTE: CATCH BASIN STRUCTURE TO BE AS DETAILED IN TYPE "C" CATCH BASIN.

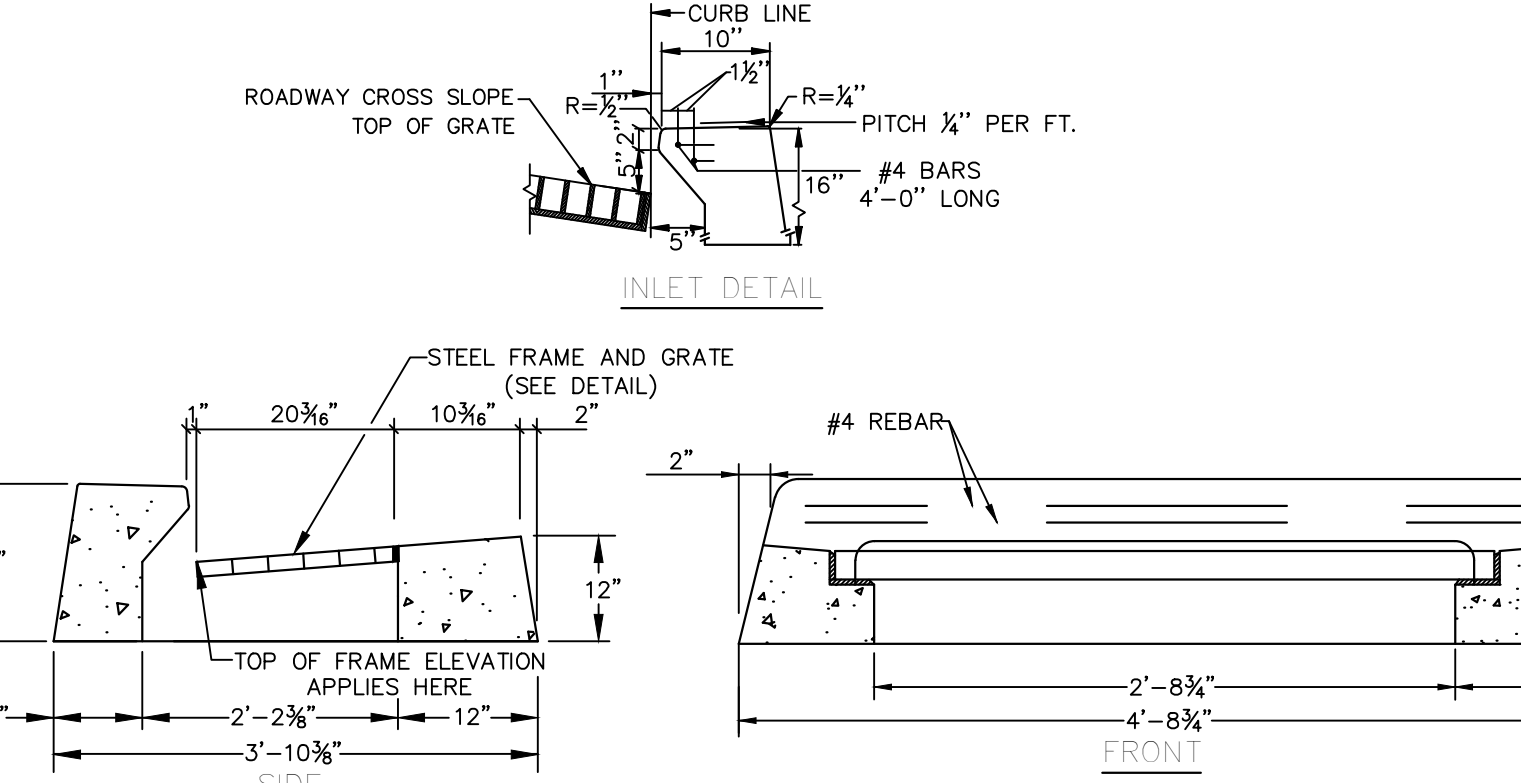


STEEL FRAME AND GRATE
NOT TO SCALE



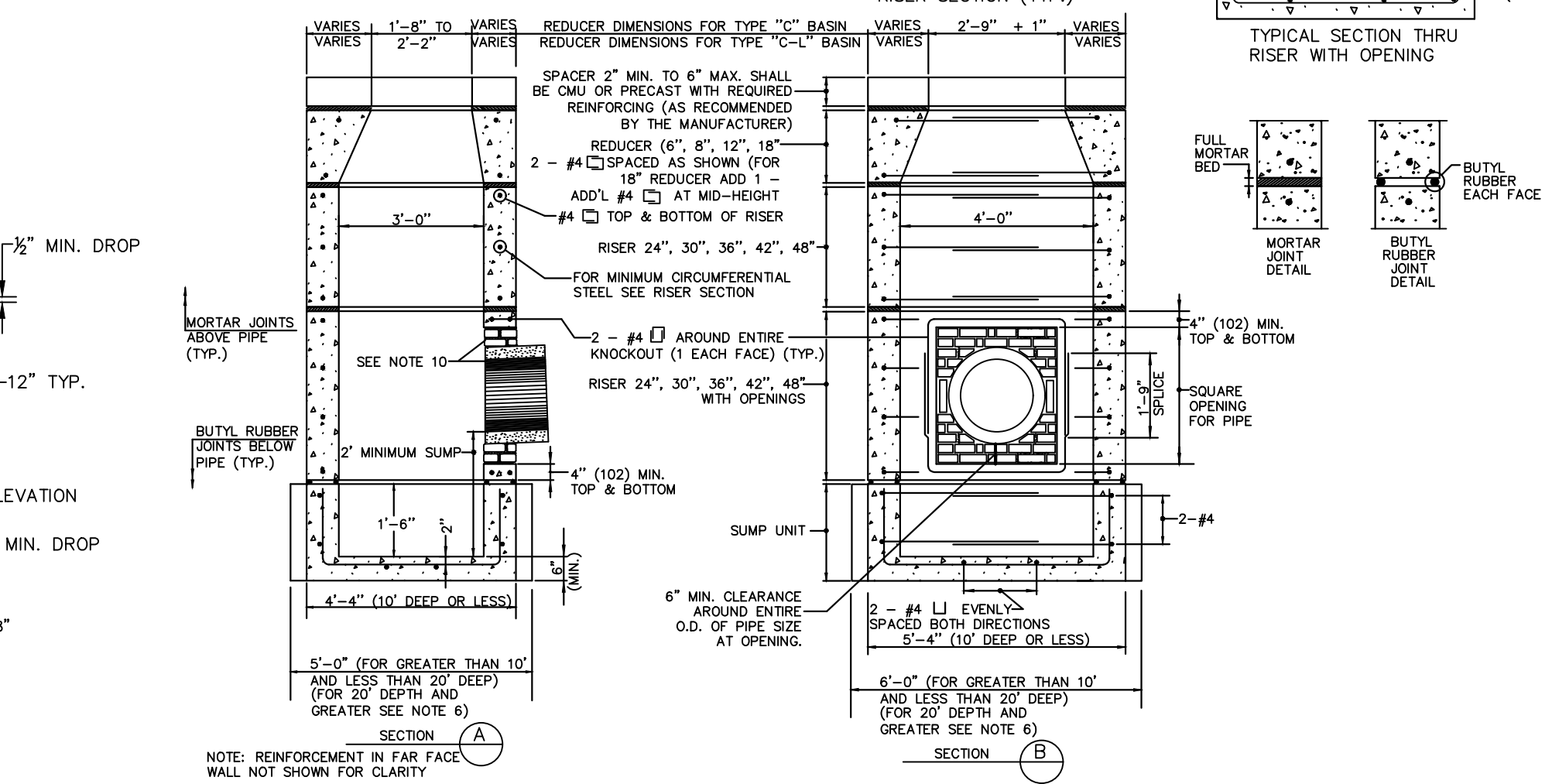
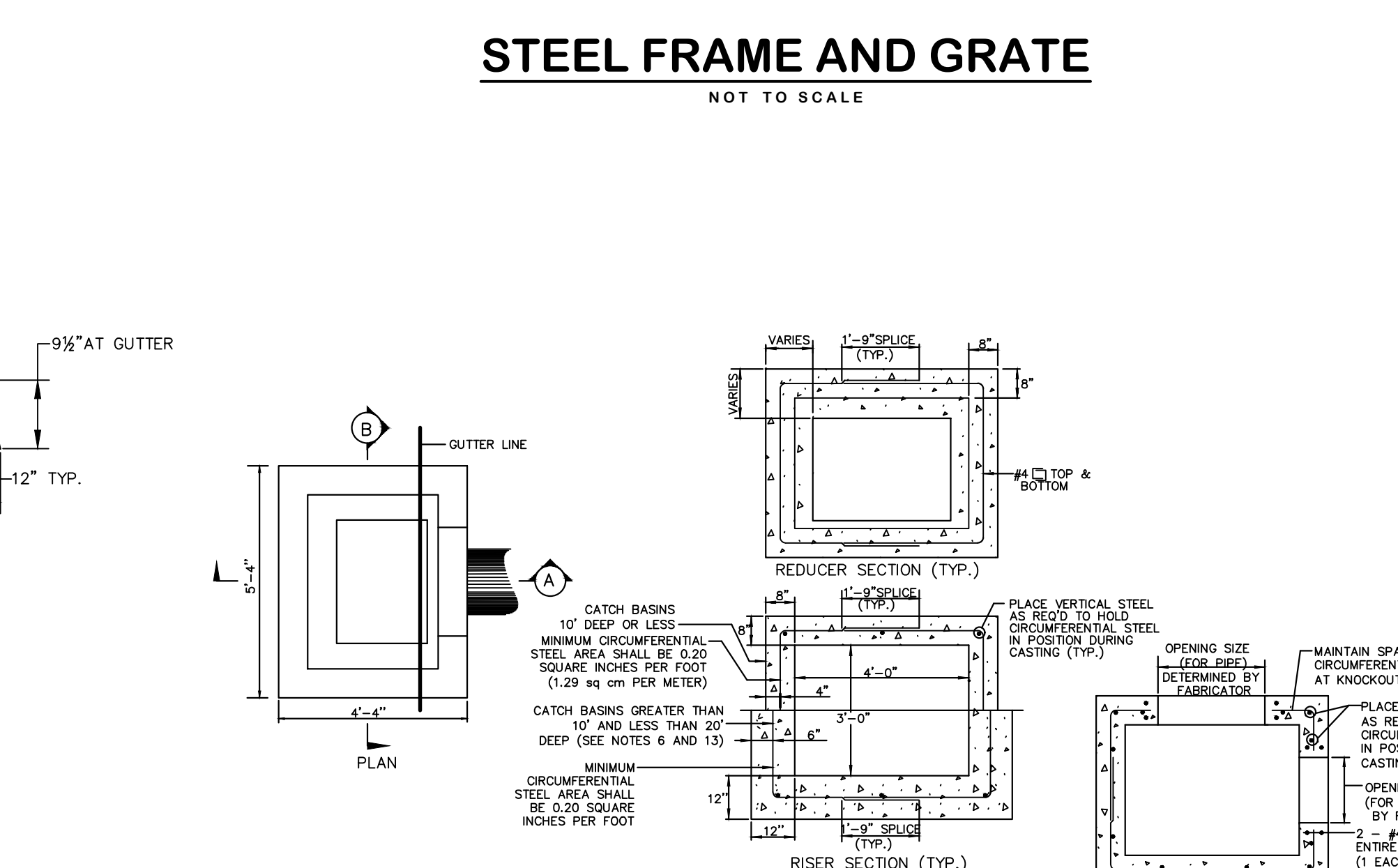
- CATCH BASIN NOTES:**
1. REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60.
 2. CATCH BASIN DETAILS SHOW STANDARD REINFORCEMENT. WELDED WIRE FABRIC WITH AN AREA EQUAL TO OR GREATER THAN THE REINFORCING SHOWN MAY BE SUBSTITUTED.
 3. ALL LAP SPLICES, DEVELOPMENT LENGTHS, BENDS FOR REINFORCEMENT, AND WELDED WIRE FABRIC SHALL CONFORM TO AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.
 4. ALL REINFORCEMENT SHALL HAVE A MINIMUM CLEAR COVER OF 2".
 5. MINIMUM CONCRETE COMPRESSIVE STRENGTH $F_c' = 4000$ PSI SHALL BE OBTAINED PRIOR TO SHIPPING.
 6. BASES AND RISERS AT A DEPTH OF 20" OR MORE SHALL BE DESIGNED BY THE CONTRACTOR AND WORKING DRAWINGS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW.
 7. SEE APPROPRIATE DETAIL FOR CATCH BASIN FRAMES AND GRATES.
 8. RISERS MAY BE PREFABRICATED WITH PIPE OPENINGS IN ALL FOUR WALLS. ADEQUATE REINFORCEMENT AROUND PIPE OPENINGS CONFORMING TO THESE PLANS SHALL BE PROVIDED. ANY RISERS USED WHERE A PIPE OPENING IS TO REMAIN IN PLACE, MUST BE FORMED UP WITH BRICK AS DIRECTED BY THE TOWN.
 9. RISERS SHALL NEVER HAVE CORNER PIPE ENTRIES.
 10. ALL OPENINGS SURROUNDING PIPES SHALL BE CLOSED USING CEMENT RUBBLE MASONRY CONFORMING TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M6.06. IF THE TOWN DETERMINES THAT THE CLOSURE OF ANY PIPE OPENING IS UNSATISFACTORY, THE CONTRACTOR SHALL IMMEDIATELY RECLOSE SAID OPENING. THE LOCATION OF PIPE OPENINGS SHALL NOT REDUCE THE WALL THICKNESS.
 11. THE LATEST CONNECTICUT D.O.T. STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
 12. WALL THICKNESS OF ALL CATCH BASINS OVER 10' DEEP SHALL BE INCREASED TO 12" THICK. INSIDE DIMENSION SHALL REMAIN THE SAME (THE 12" THICKNESS WILL START AFTER THE FIRST 10').
 13. BUTYL RUBBER JOINT SEAL SHALL CONFORM TO AASHTO M-198 AND MORTAR SHALL CONFORM TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M11.04. ALL CATCH BASINS SHALL BE BUILT WITH SPACERS AS SHOWN IN THE DETAILS.
 14. SHRINKAGE AND TEMPERATURE REINFORCEMENT SHALL BE PROVIDED IN THE TOPS OF SLABS. THE TOTAL AREA OF REINFORCEMENT PROVIDED SHALL BE AT LEAST 0.125 SQUARE INCHES PER FOOT IN EACH DIRECTION. THE MAXIMUM SPACING OF THIS REINFORCEMENT SHALL NOT EXCEED 18 INCHES.
 15. ALL CONCRETE CATCH BASIN TOPS SHALL BE CAST TO MATCH ADJACENT CURB TYPE AND HEIGHT.
 16. CATCH BASIN TOP DIMENSIONS PROVIDED ON THE PLANS ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

CATCH BASIN NOTES
NOT TO SCALE

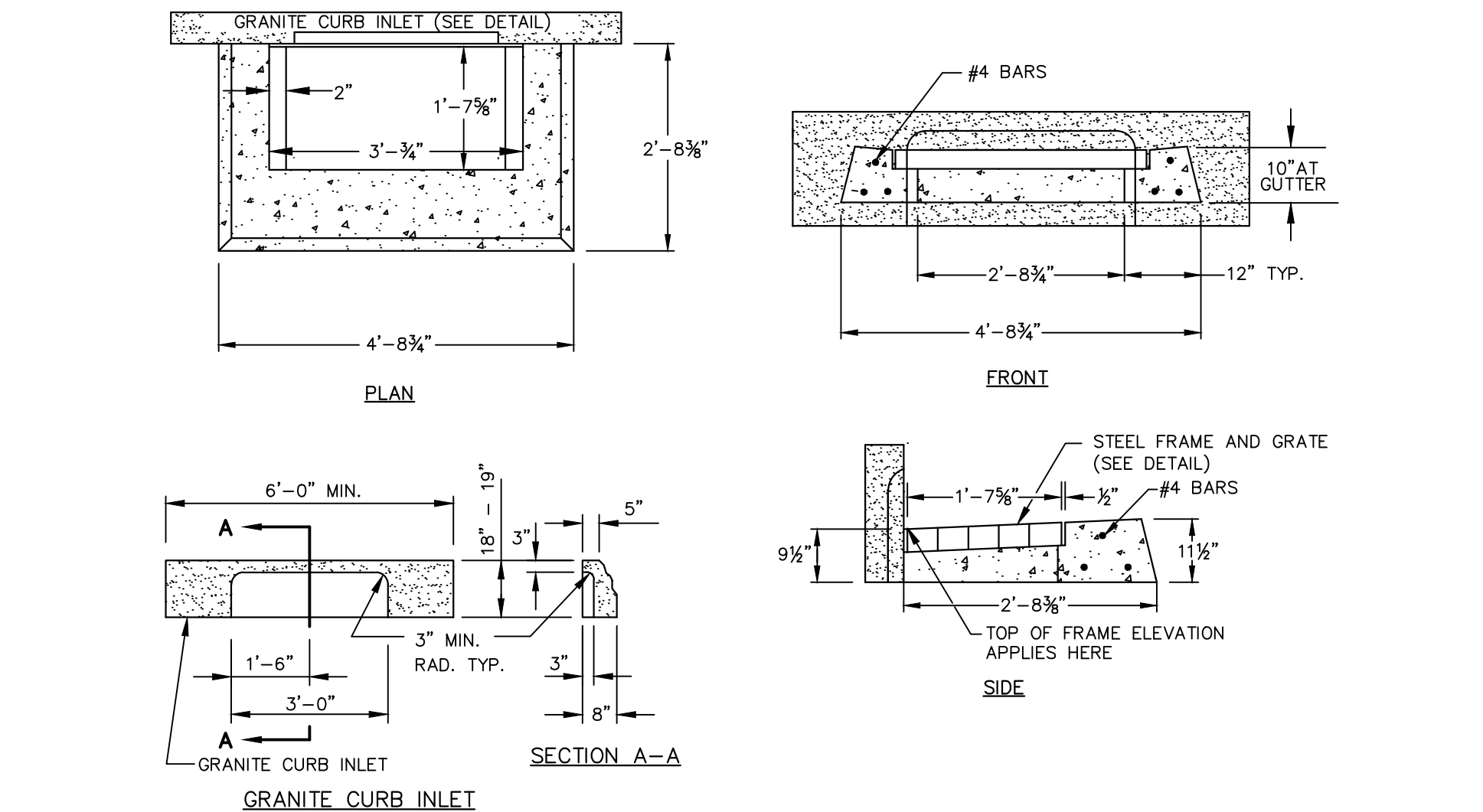


TYPE "C" CATCH BASIN TOP (FOR CONCRETE CURB)
NOT TO SCALE

NOTE: 1. CONCRETE TOP SHALL BE CAST TO MATCH ADJACENT CONCRETE CURB.
2. CATCH BASIN TOP DIMENSIONS PROVIDED ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.



TYPE "C" AND TYPE "C-L" CATCH BASIN
NOT TO SCALE



TYPE "C" CATCH BASIN TOP (FOR GRANITE CURB)
NOT TO SCALE

TYPE "C-L" CATCH BASIN TOP
NOT TO SCALE

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TOWN OF MANCHESTER
PUBLIC WORKS DEPARTMENT
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LEGEND

—	WETLANDS BOUNDARY	☆	LIGHT POLE
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—	GRADE RAIL	⊙	DECIDUOUS TREE
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—	STOCKADE FENCE	⊖	DRAINAGE MANHOLE
—	WIRE FENCE	⊗	CATCH BASIN
—	CHAIN LINK FENCE	⊕	CULVERT END
—	PROPERTY LINE	⊖	HYDRANT
—	RAILROAD TRACKS	⊗	WATER VALVE
—	SILT FENCE	⊕	WATER STOP
—	CONCRETE MONUMENT	⊖	GRANITE MONUMENT
—	GRANITE MONUMENT	⊗	BUTTERFLY VALVE
—	IRON PIPE	⊕	BLOW OFF
—	IRON ROD	⊖	SIGN
—	DRILL HOLE	⊗	DOUBLE POST SIGN
—	UTILITY POLE	⊕	MAIL BOX
—	TRAFFIC SPAN POLE	⊖	BOLLARD
—	ELECTRIC FLAG	⊗	CONTROL CABINET
—	WETLAND FLAG	⊕	GAS GATE
—	CATV TUBE	⊖	TELEPHONE BOX

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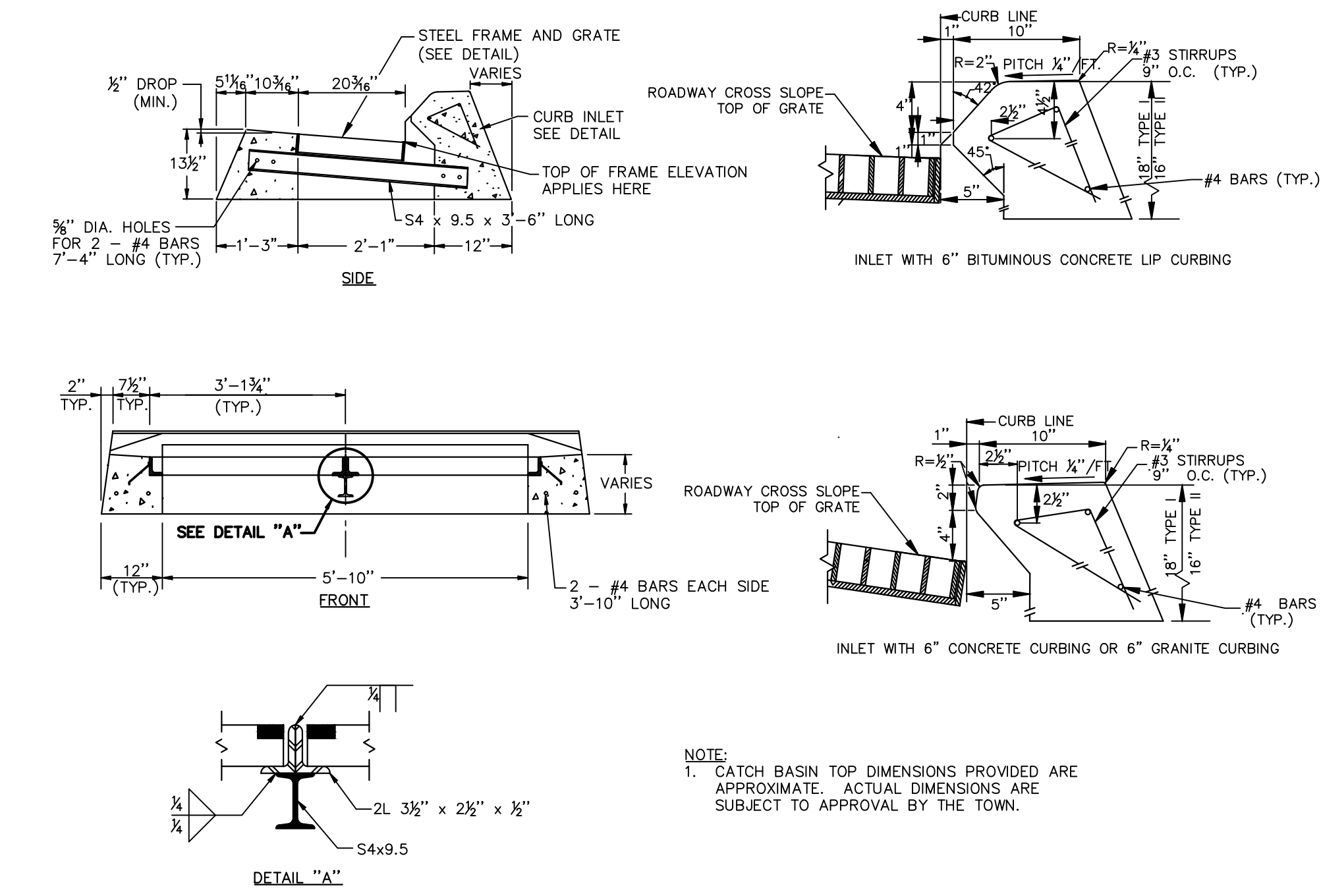
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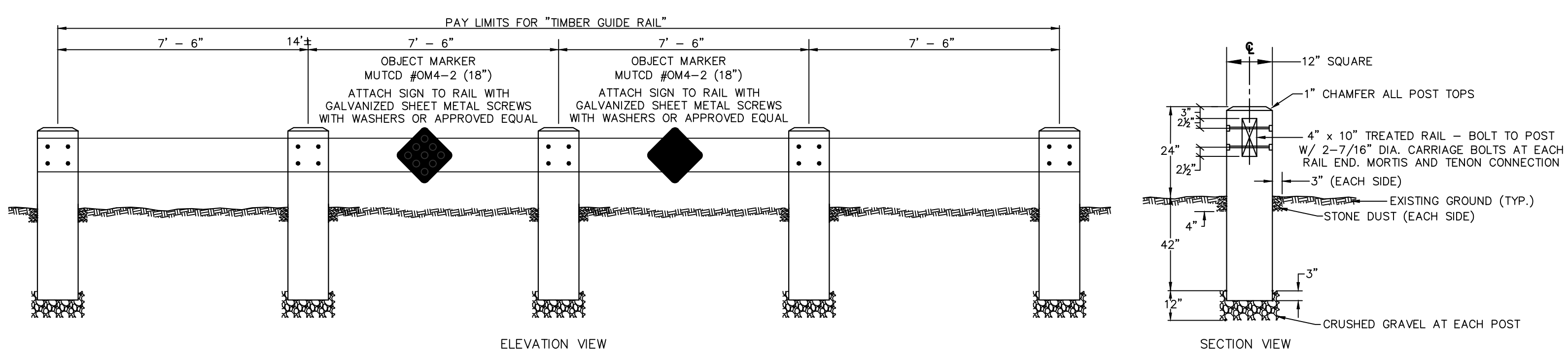
PROJECT TITLE
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-GRISWOLD STREET
AREA IMPROVEMENTS**

SHEET TITLE
DETAILS

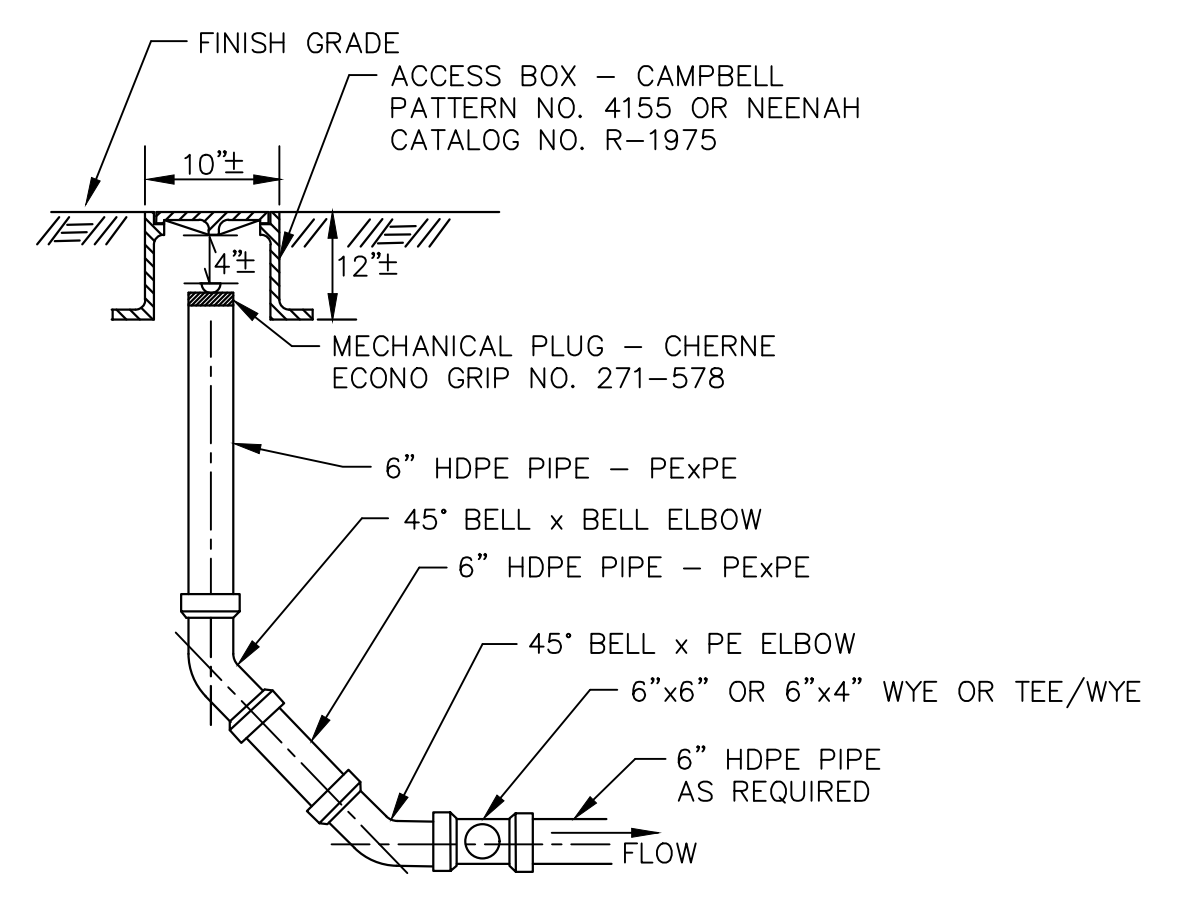
SHEET NUMBER
20 of 20



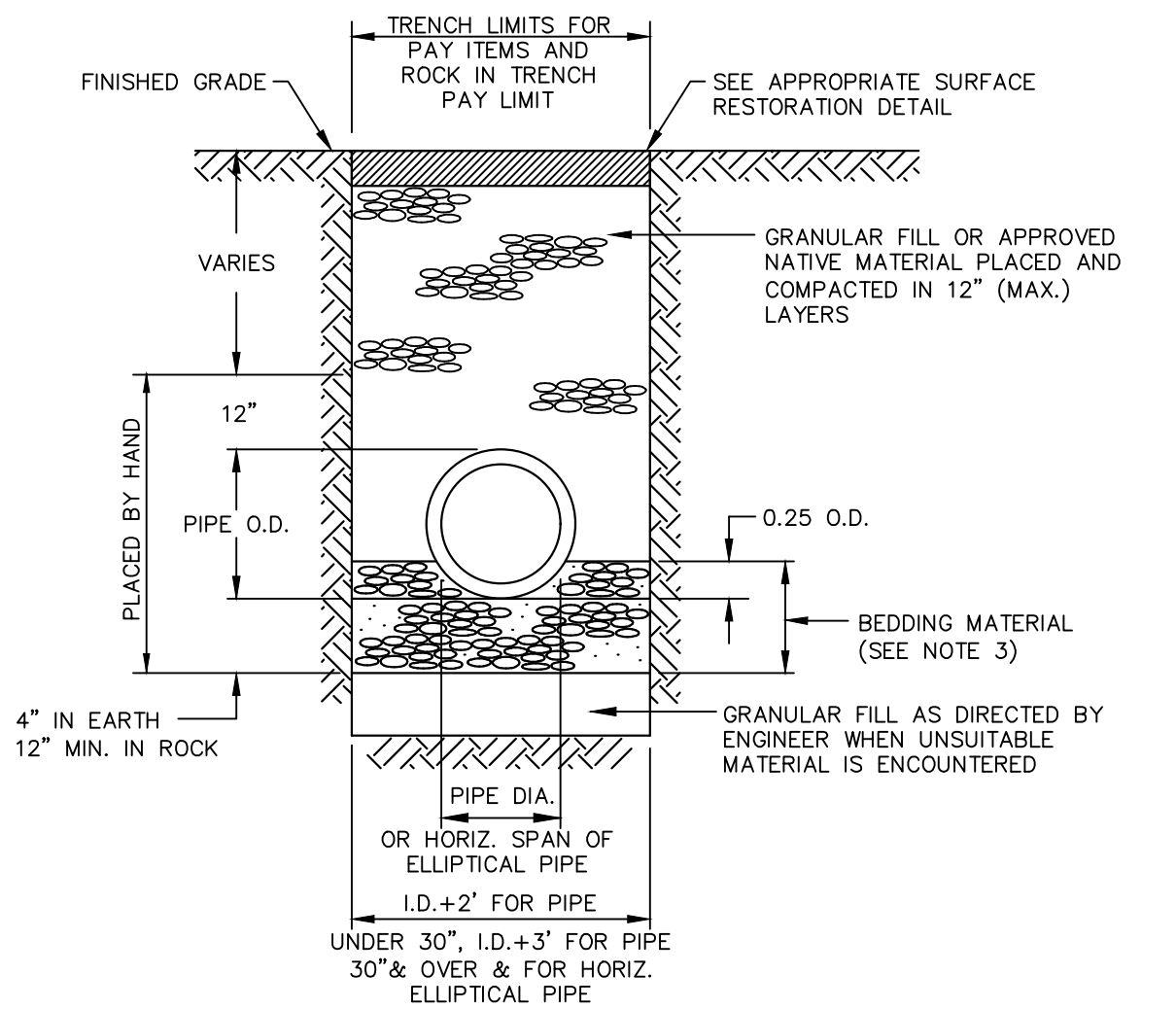
TYPE "C" DOUBLE GRATE TYPE II CATCH BASIN TOP
(FOR BITUMINOUS CONCRETE LIP CURB, CONCRETE CURB AND GRANITE CURB)
NOT TO SCALE



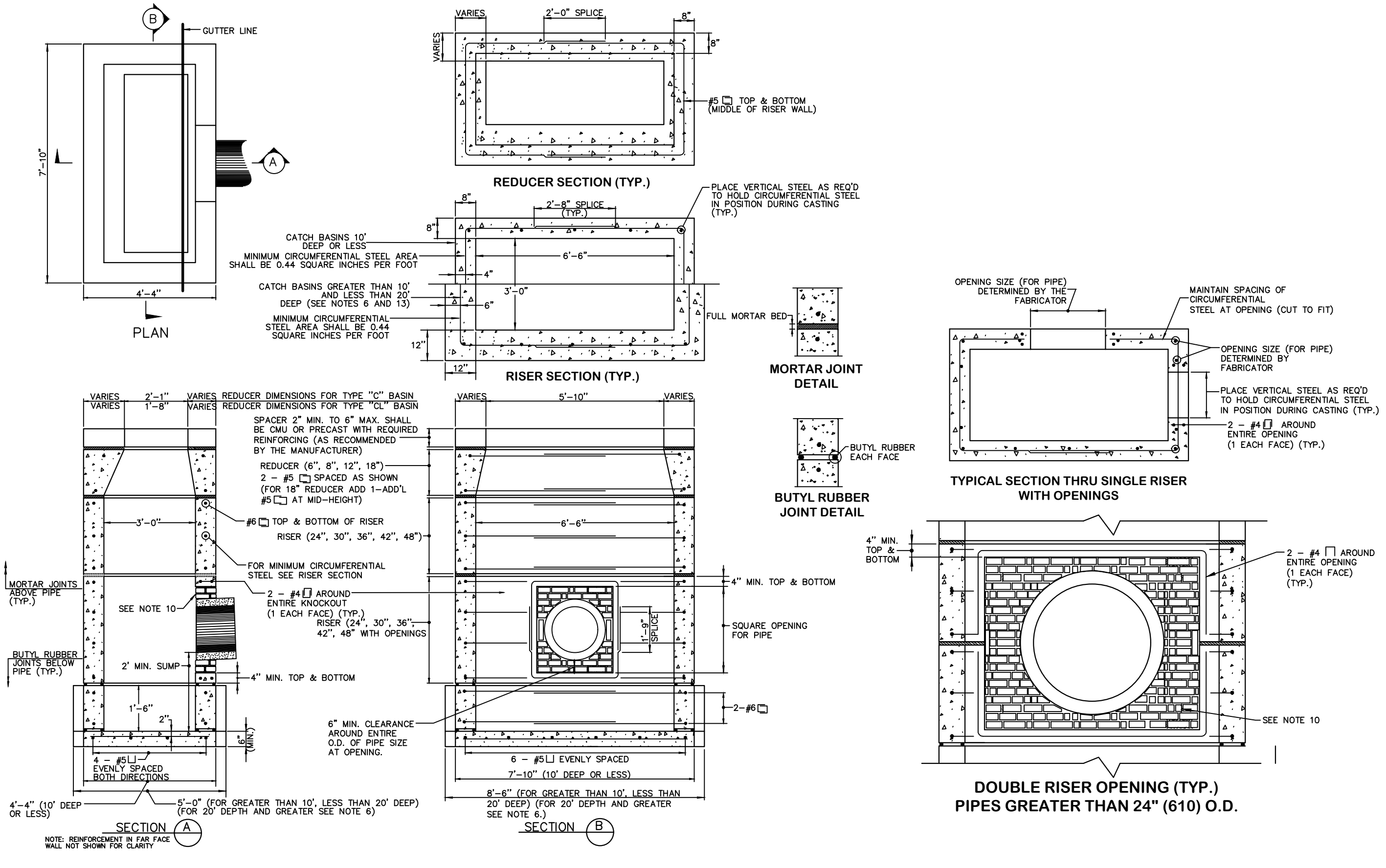
TIMBER RAIL
NOT TO SCALE



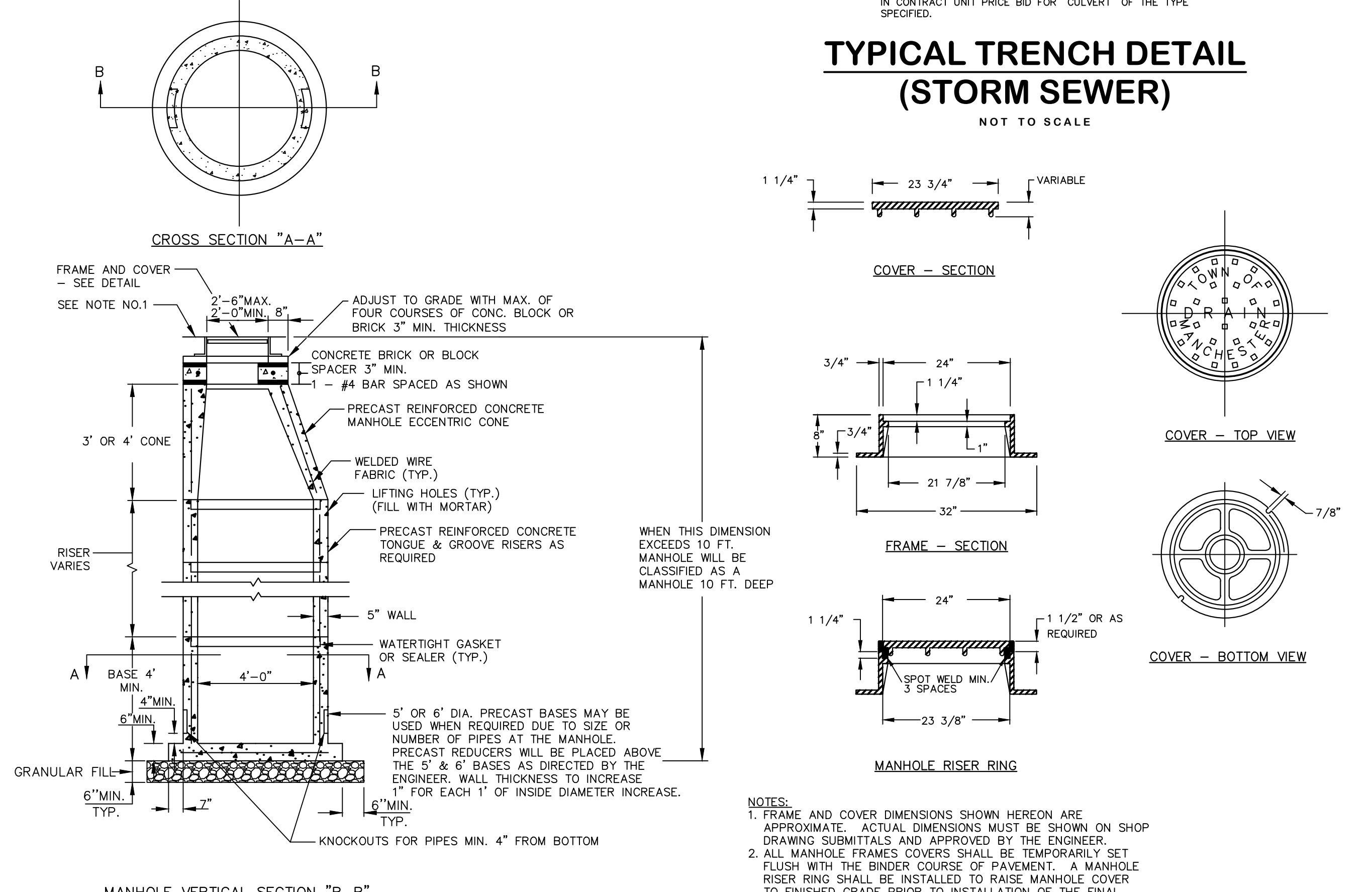
STORM SEWER CLEANOUT
NOT TO SCALE



TYPICAL TRENCH DETAIL (STORM SEWER)
NOT TO SCALE



TYPE "C" AND "C-L" DOUBLE GRATE TYPE II CATCH BASIN
NOT TO SCALE



STORM MANHOLE
NOT TO SCALE

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